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Introduction

# 1.1 The Project

Lock 16 - Scotland’s Centre of Excellence for Canals & Traditional Skills (Lock 16) is located next to the Forth and Clyde Canal in Camelon, Falkirk, to the west of the town centre. Lock 16, a partnership project between Scottish Canals (SC) and Historic Environment Scotland (HES) is posed to become a hub canal based innovation, pioneering canal management and maintenance as well a traditional and heritage skills training and local employment.

The current scheme proposes facilities for SC and HES while allowing flexibility for further discussions on shared spaces within the building.

The project forms part of the Great Places theme of the Falkirk and Grangemouth Growth Deal. The project will provide much-needed regeneration to a canal-side asset – the former A.G Barr factory. The design principle is based on minimal intervention in the existing structure by focusing on repurposing the existing building. Moreover, the proposal involves adopting a responsible whole-life approach to sustainability, tailored to ensure the delivery of valuable socially inclusive, environmentally sensitive, and local economic growth outcomes during the delivery and operational stages and beyond.

SC has entered into a Pre-Construction Services Agreement (General Contractor) 2019 Edition with Robertson Construction to develop designs which includes workshops, classroom/ training spaces and support functions alongside a facility for SC to conserve, operate, maintain and enhance the Scheduled Ancient Monuments that form the lowland canal network.

This report confirms objectives, develops the project brief, and evolves the initial design concept into a more detailed proposal, taking into account key requirements established by SC and HES, existing building/site constraints, budgetary constraints, and the agreed scope of works. The document also evaluates key design decisions and any significant challenges in collaboration with Robertson Construction, end-user clients, and the project’s wider design team.



1.2 The Team

Client:



Client Consultants:



Robertson Design Team:



Client Project Partner:





2

Site Information



## 2.1 Location

The site’s strategic placement along the Forth and Clyde Canal, positioned between the Falkirk Wheel and the recently refurbished Rosebank Distillery, renders it a complementary component of the broader ambition to establish a canal-side route connecting these two prominent visitor destinations. This positioning offers a convenient resting point for pedestrians and cyclists journeying along the canal path. The broader vision for the public realm surrounding Lock 16 entails substantial enhancements to pedestrian walkways and the improvement of local green spaces, thereby fostering the creation of safe and enjoyable routes.



Lock 16 Location



The site has a long history of industrial use and until the early 20th century, was part of a larger foundry site (originally the Union Foundry). It was adjacent to the Portdownie canal basin, one of the basins that connected the Forth and Clyde and Union Canals.

SC purchased the building in 2007, and in 2019/20, undertook fabric improvements to make the building wind and water-tight and address issues of lead paint, asbestos, and the impact of building vandalism.







View from Lock 16, Camelon Circa 1920



Bascule bridge Lock 16, Camelon. Union Inn to the left out of frame. Circa Early 1960s



Lock 16 and Forth Canal with Union Inn present Early 20th Century



Row of cottages and gardens at Cherry Lane, Tamfourhill. Forth and Clyde Iron Works in the background Circa 1960 just before demolition. Union Inn to the right out of frame. Construction of the Minerals Water Factory briefly follows





The site had a long history of industrial use until the early 20th century. A wider sense of history can be found within the surrounding context as the Forth and Clyde Canal and the Antonine Wall, which run to the south of the site, are both scheduled monuments.

**KEY**

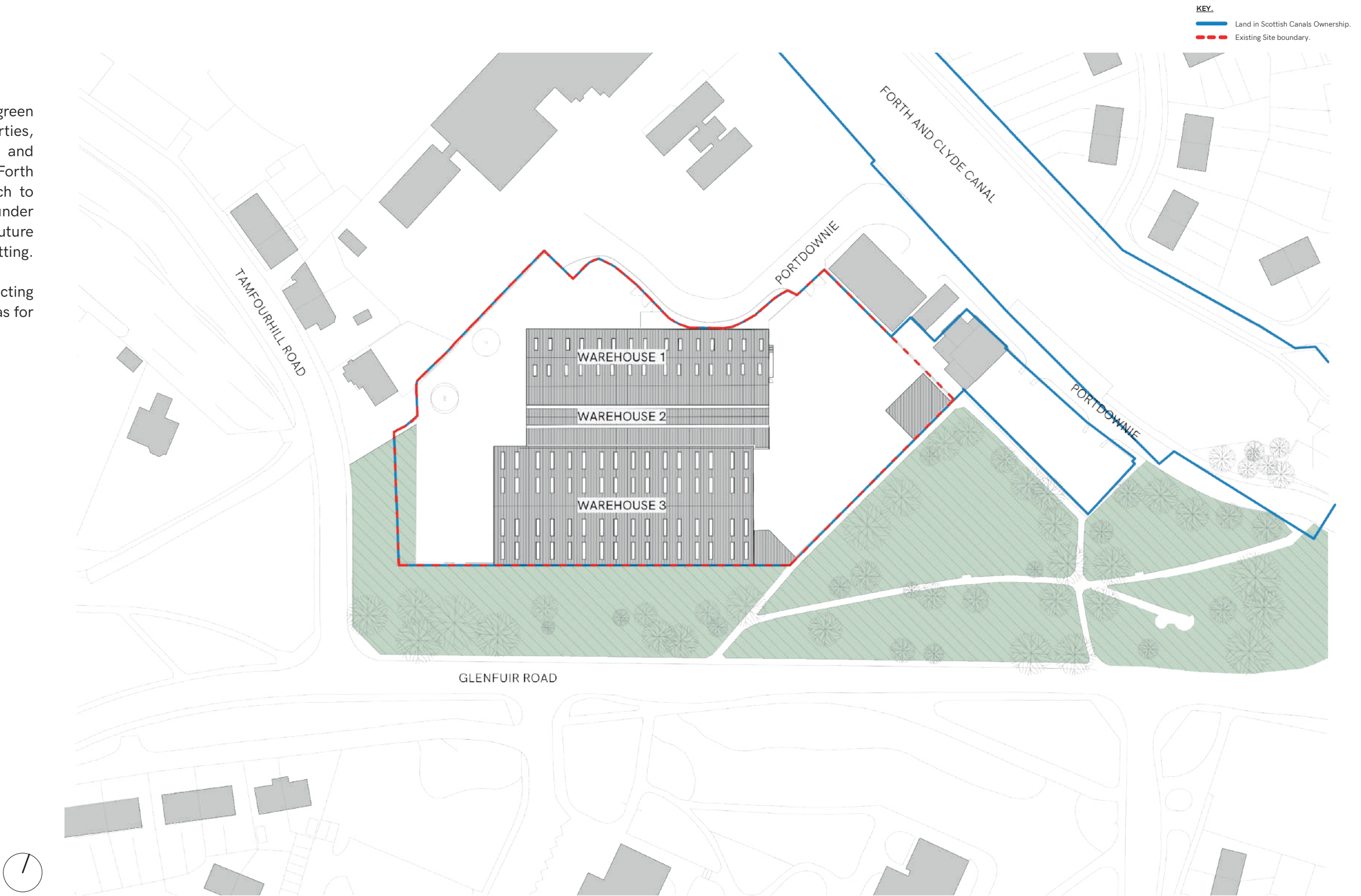
- Historic Antonine Wall
- Former route of the Union Canal
- Existing Site Boundary

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### 2.3 Existing Site

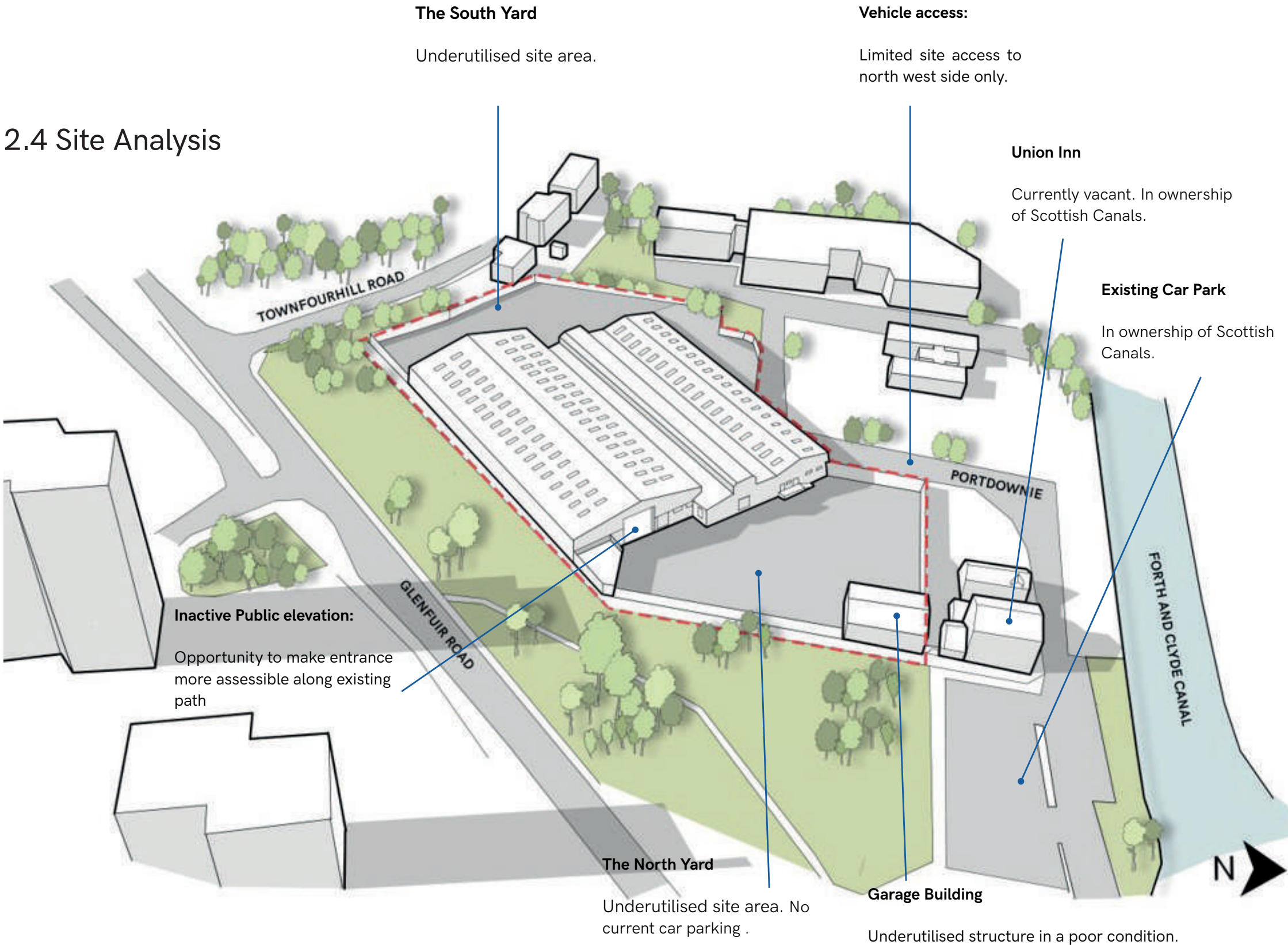
The existing site is surrounded by a diverse mix of green space, roads, commercial units, and residential properties, creating a unique and varied context. To the north and east, SC own a substantial area of land, including the Forth & Clyde Canal, which provides an impressive approach to the site. The green space to south, which is currently under ownership of Falkirk Council, is protected from future development and this will preserve the building setting.

Currently, the land surrounding the site is fenced off, restricting public access, and remains vacant, offering a blank canvas for development.





2.4 Site Analysis



North Yard



South Yard



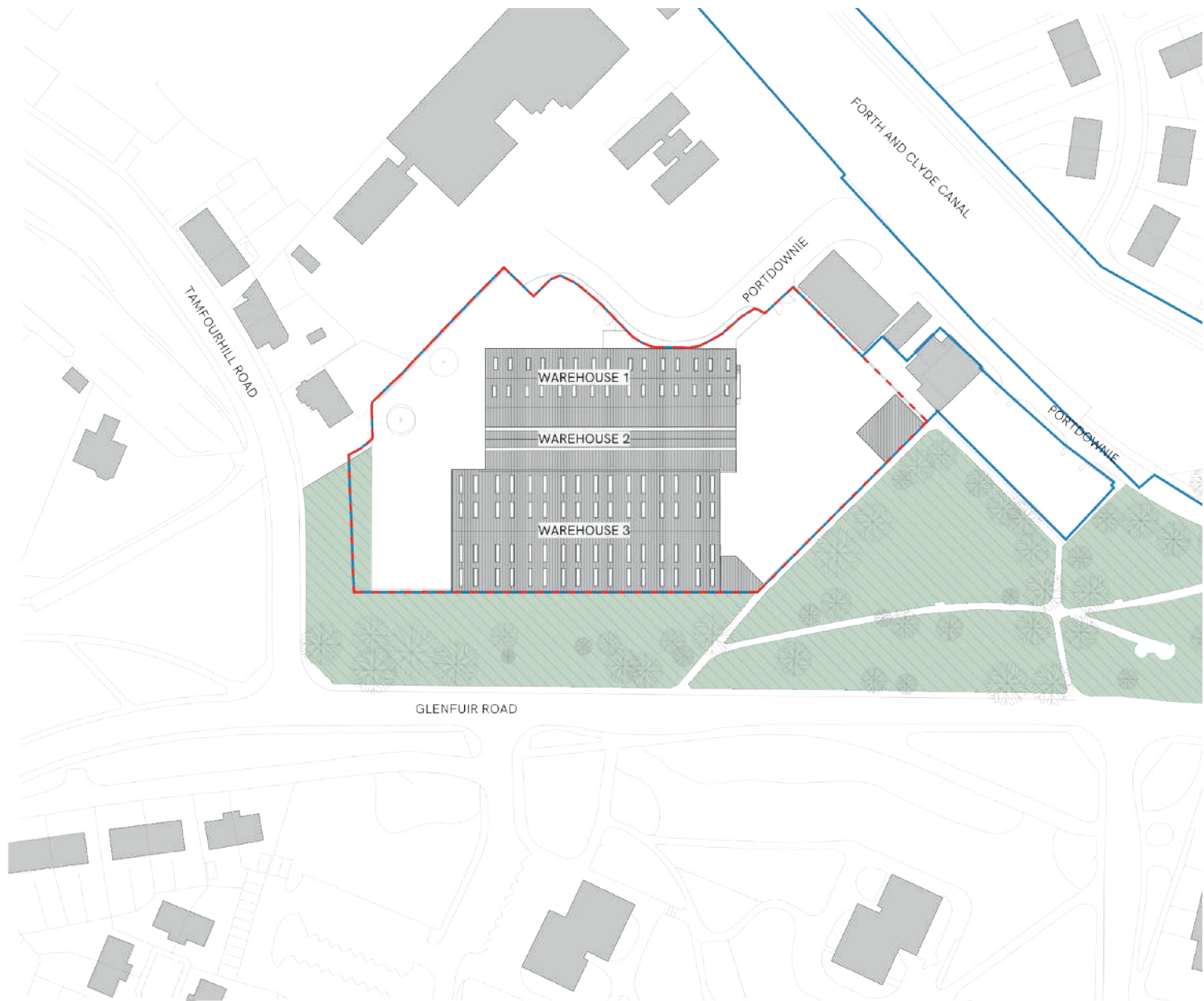
Site from Glenfuir Road



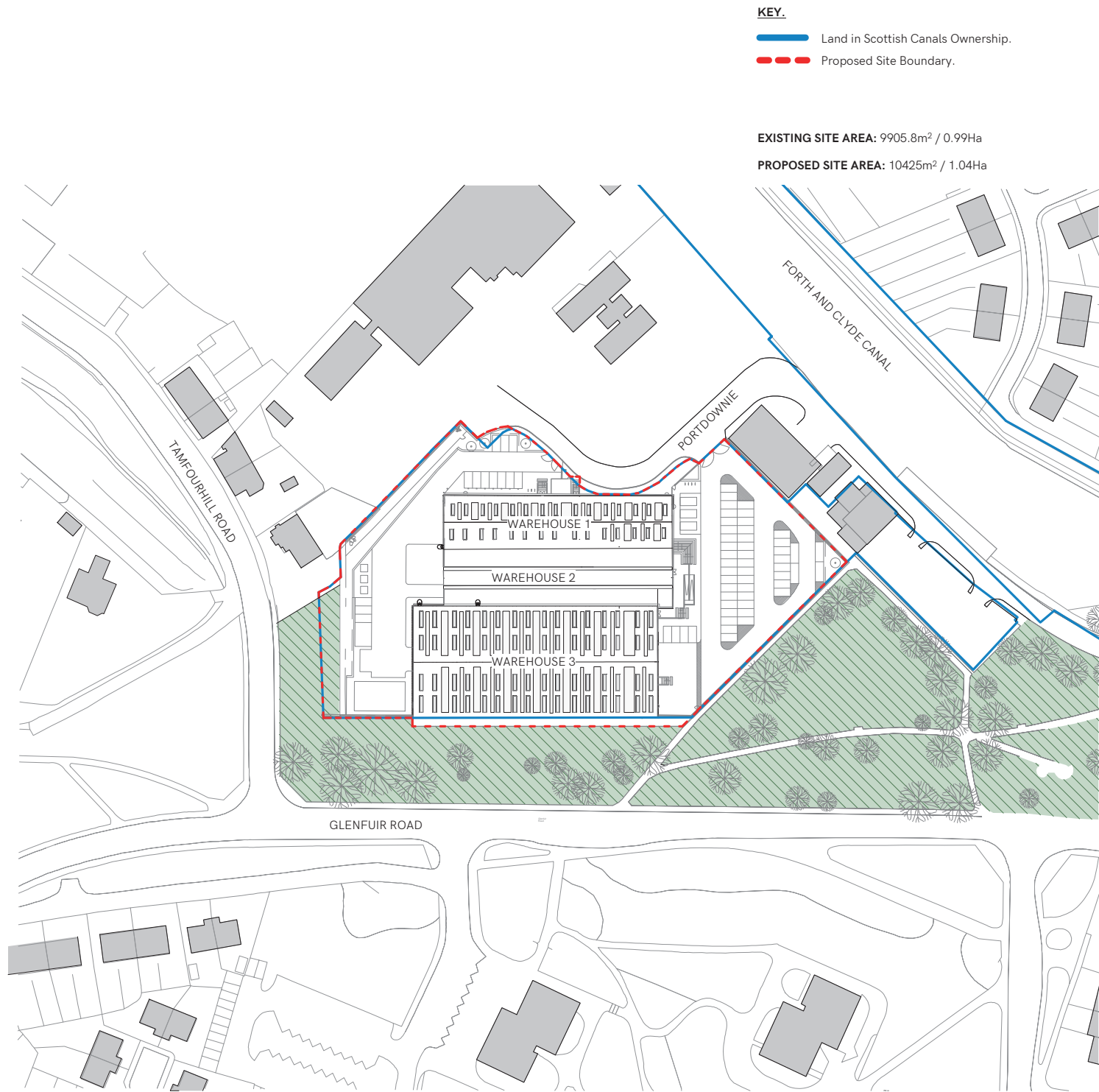
## 2.5 Site Boundary

SC currently own the Forth and Clyde Canal and several properties adjacent to the proposed development site. At present, all vehicles, cyclists, and pedestrians access the site via the entrance off Portdownie however the proposed upgrades to the boundary will permit access via foot and cycle along the existing public footway from Glenfuir Road. This new means of access also improves connectivity to SC nearby property.

The proposed transfer of land, as indicated by the images below, is required to facilitate escape from the building.



Existing Site Plan



Proposed Site Plan



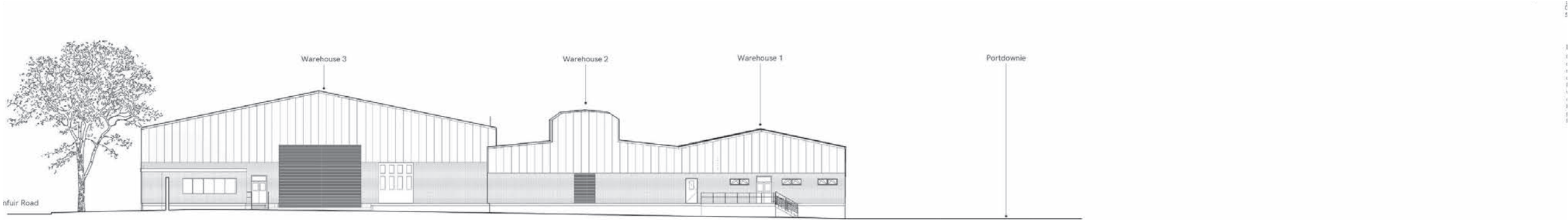
## 2.6 Site Survey

The following surveys have been carried out on the existing site:

- Topographical Survey
- Asbestos Survey
- Utility Survey
- Ecology survey
- Transport
- Noise Impact Assessment
- Tree survey and Arboricultural Impact Assessment



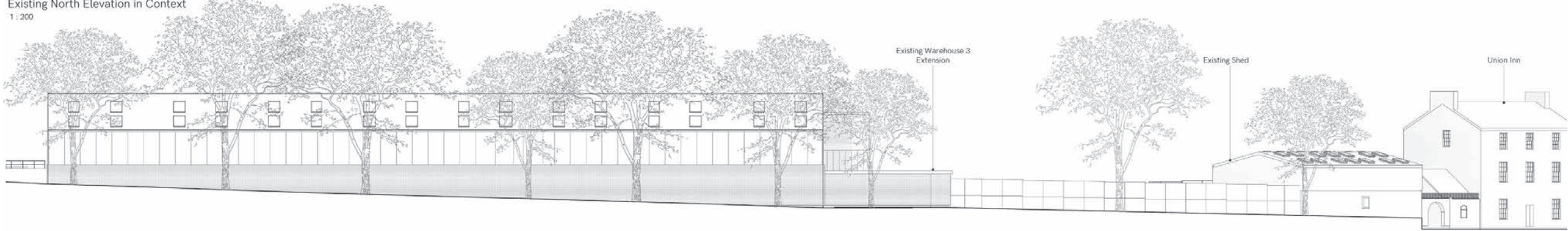




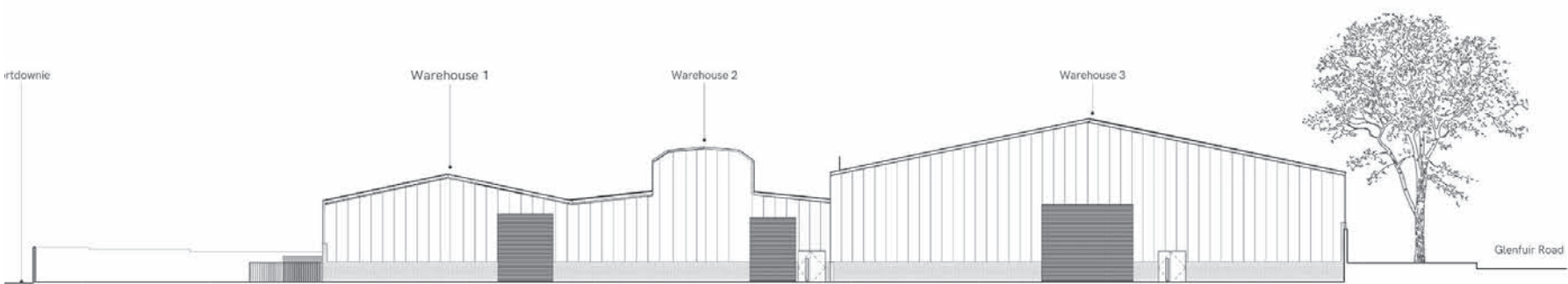
Existing East Elevation in Context  
1 : 200



Existing North Elevation in Context  
1 : 200



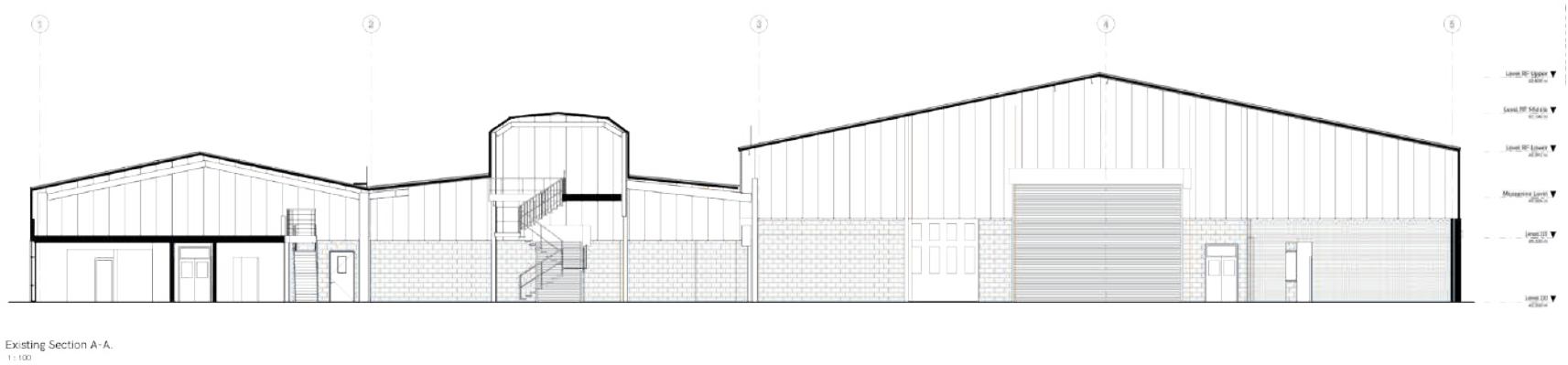
Existing South Elevation in Context  
1 : 200



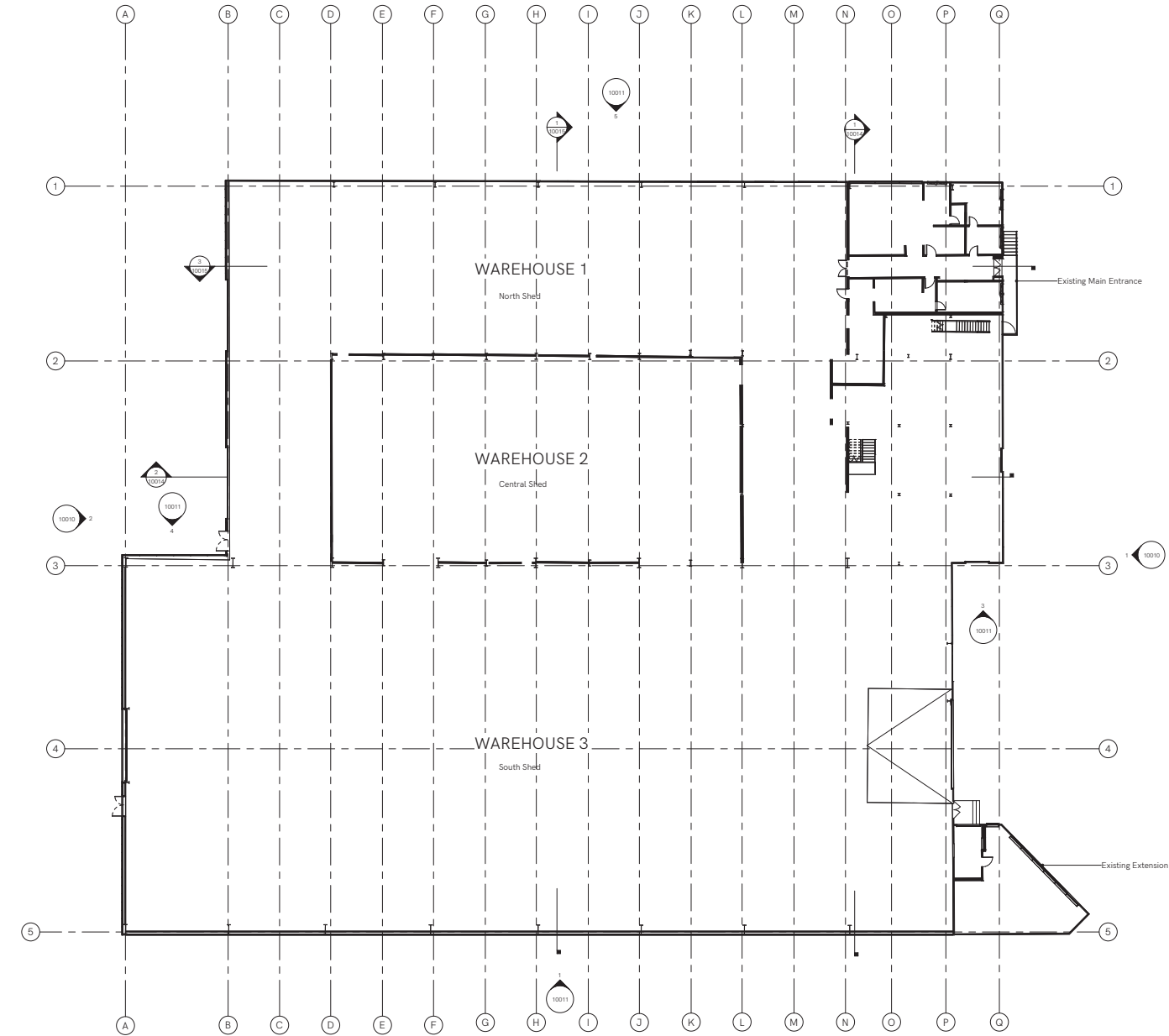
Existing West Elevation in Context  
1 : 200

# 2.8 Existing Building

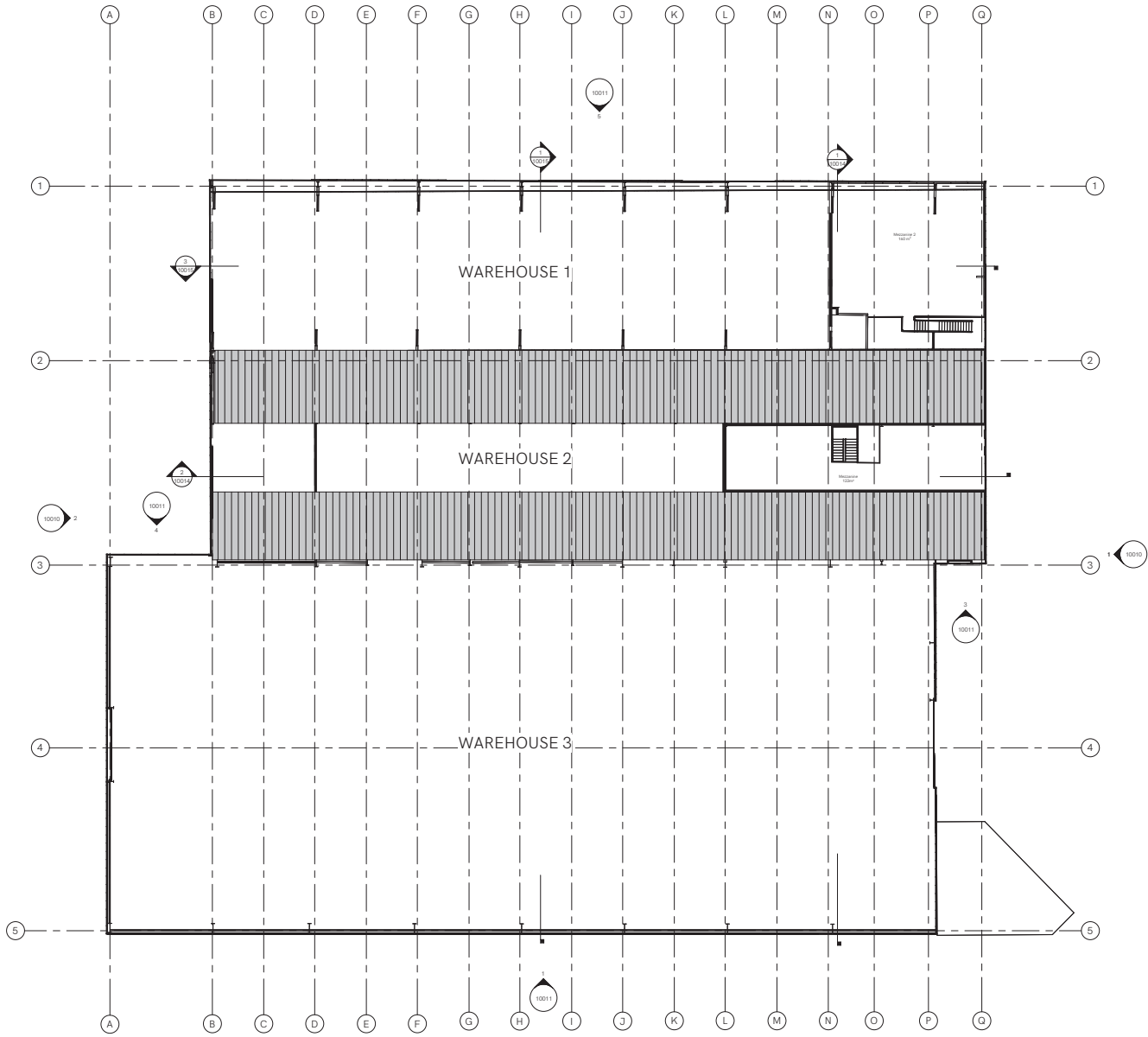
The existing building consists of three distinct warehouse spaces, each varying in scale and form, offering a variety of options for the internal layout. The industrial aesthetic, which flows seamlessly from the exterior to the interior, is well-suited for the proposed workshop spaces, creating an environment that encourages creativity. The most recent modifications to the building shell includes new rooflights which flood the expansive warehouse areas with natural light, enhancing the overall atmosphere. Currently vacant, the building presents a significant opportunity for development, offering considerable potential to be transformed into a dynamic and versatile workspace that can cater to a range of uses.



Existing Section



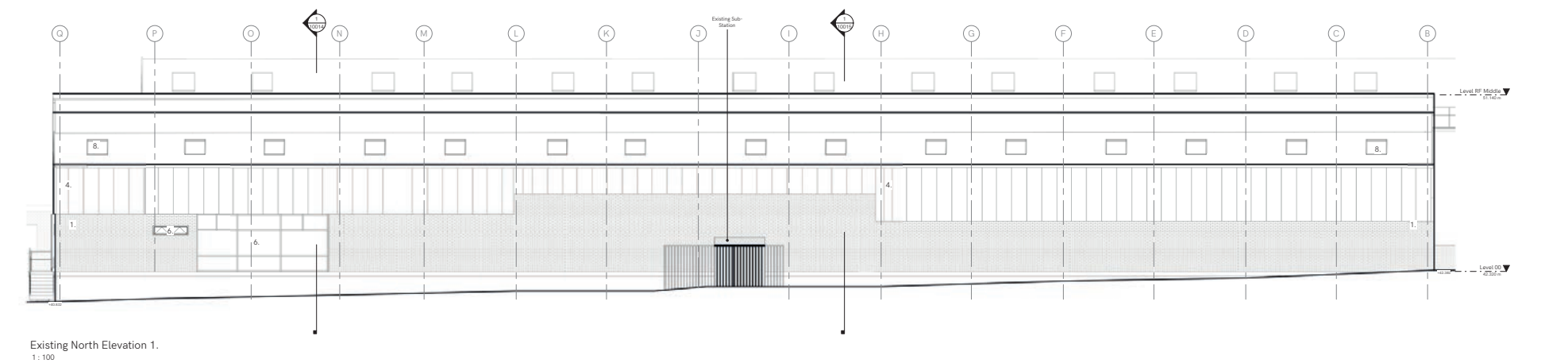
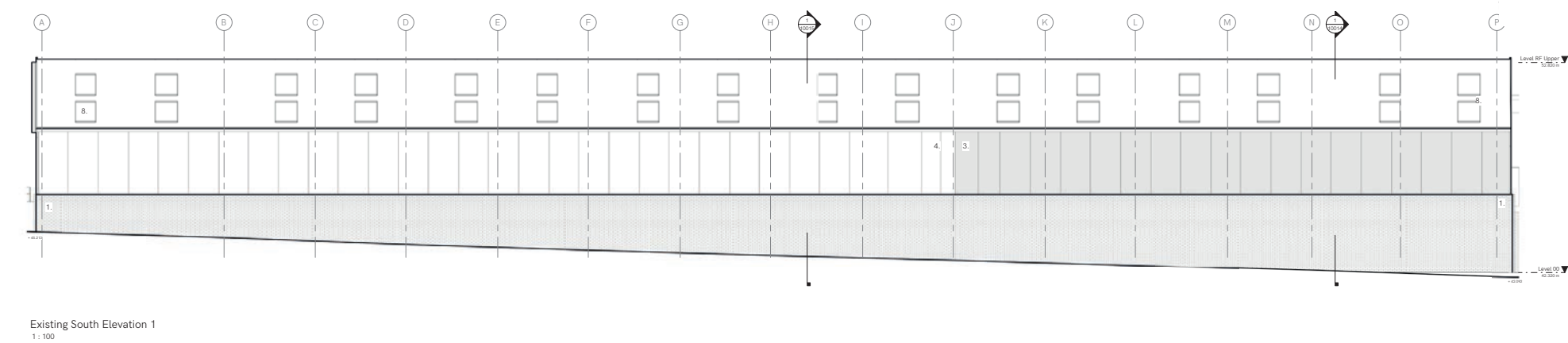
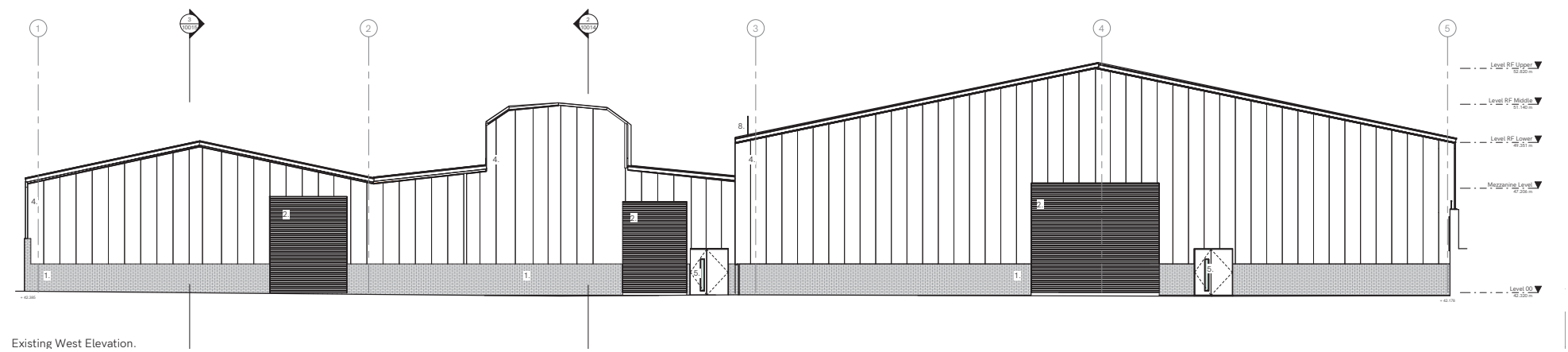
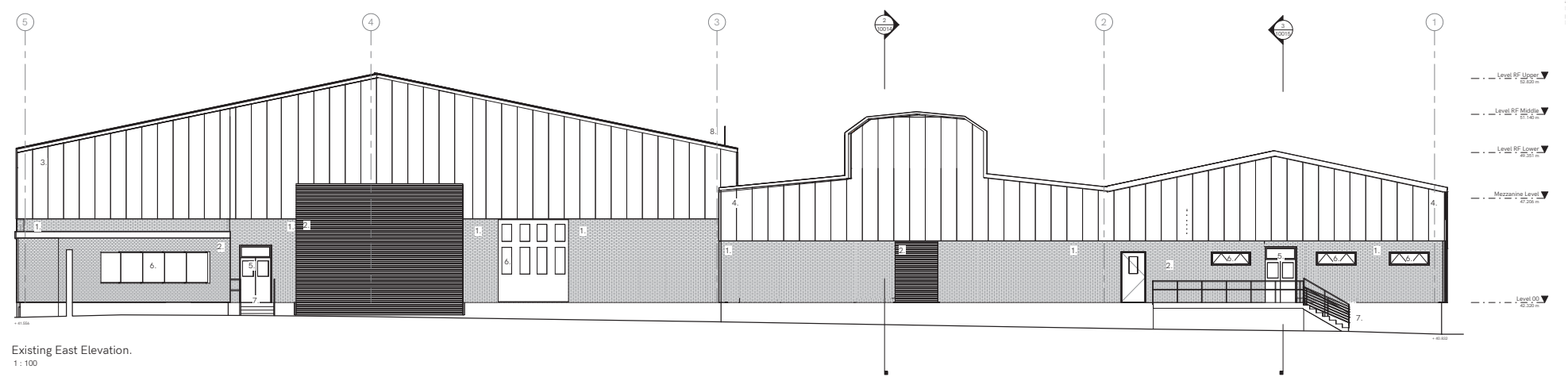
Existing Ground Floor Plan



Existing Mezzanine Plan

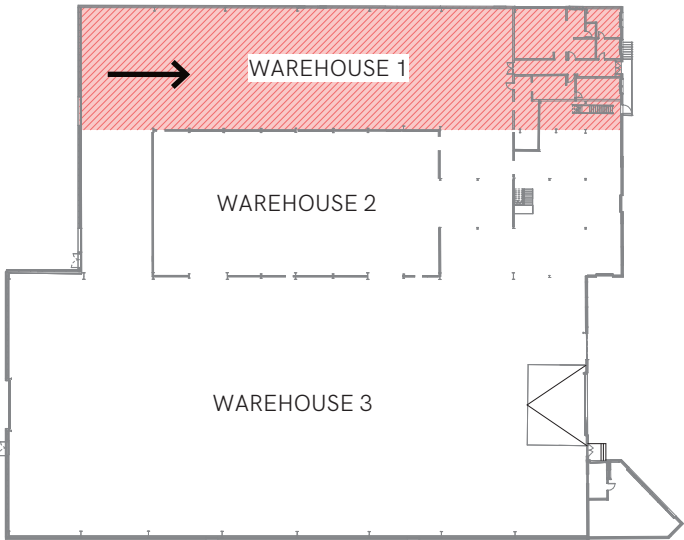
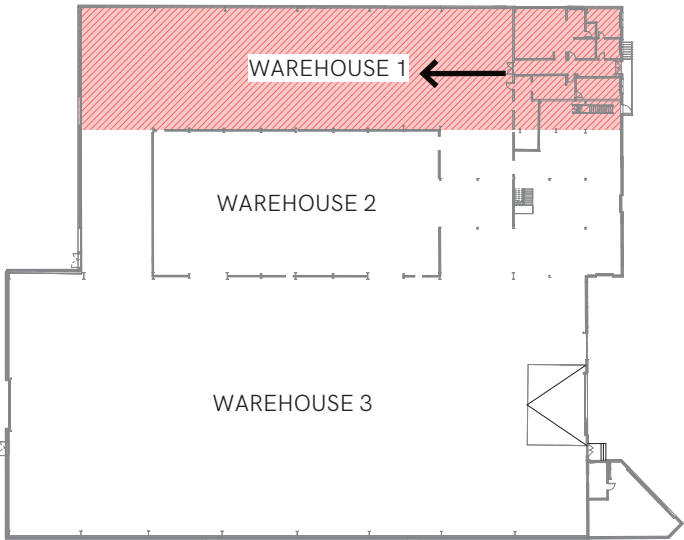
The existing building features an industrial aesthetic, which was well-suited to its previous private use. The facades are composed of a combination of brickwork, polycarbonate panels, insulated trapezoidal cladding, roller shutters, and areas of glazing. To the North elevation there are varying site levels which are considered in the development proposal meanwhile and small extension of the building is in visibly poor condition and is therefore proposed for removal to allow for better use of the external space. In addition, the current entrance, which is also in a state of disrepair, does not provide a welcoming or functional approach to the building and to address this, a new entrance will be created to improve both accessibility and aesthetics.

To the South of the site the levels are more consistant along the building elevation where there are large roller shutters. The privacy of this side of the building will be retained as it will be for authorised access only.



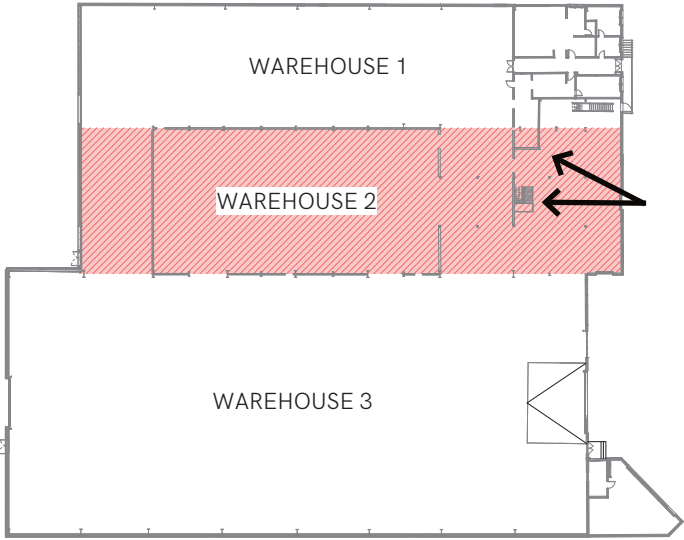
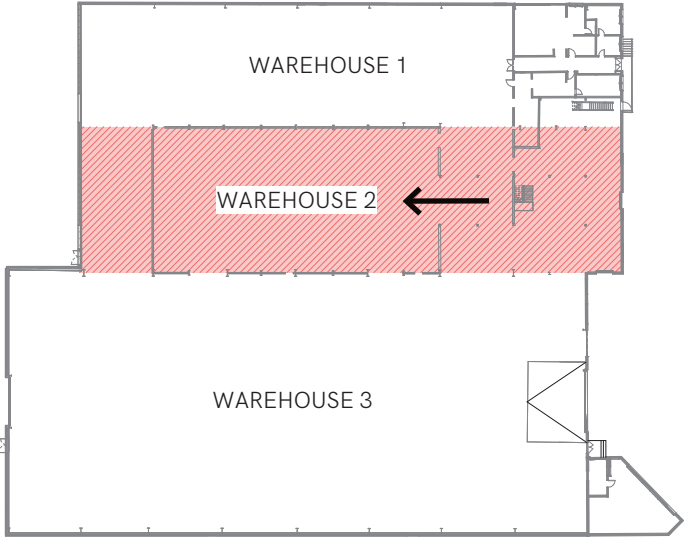


Existing Warehouse 1



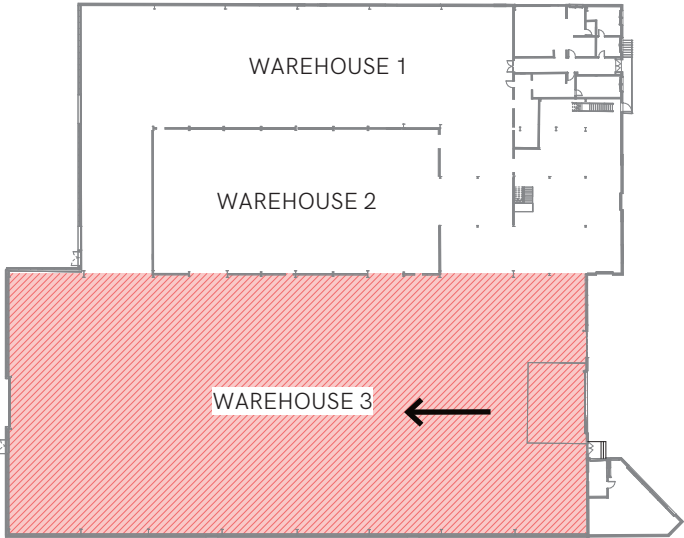
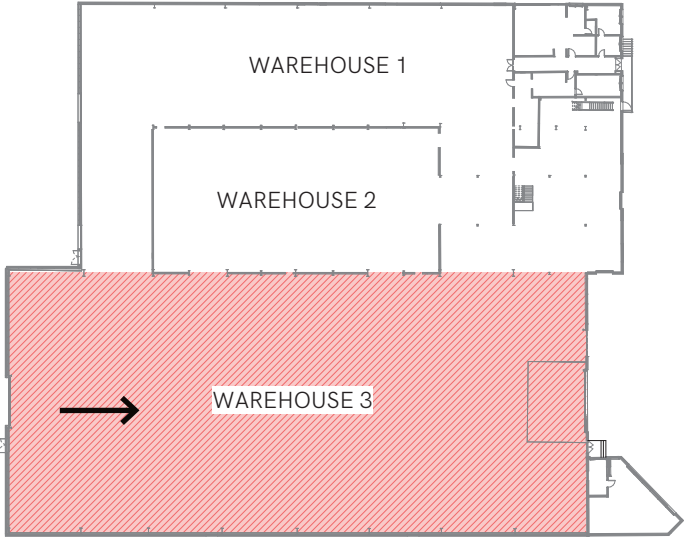


Existing Warehouse 2



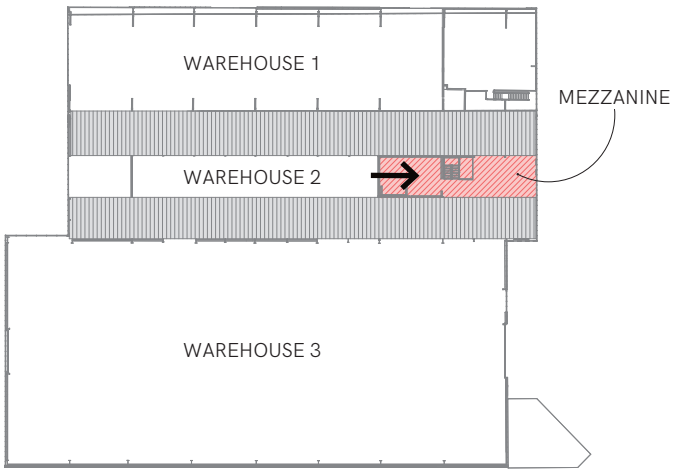
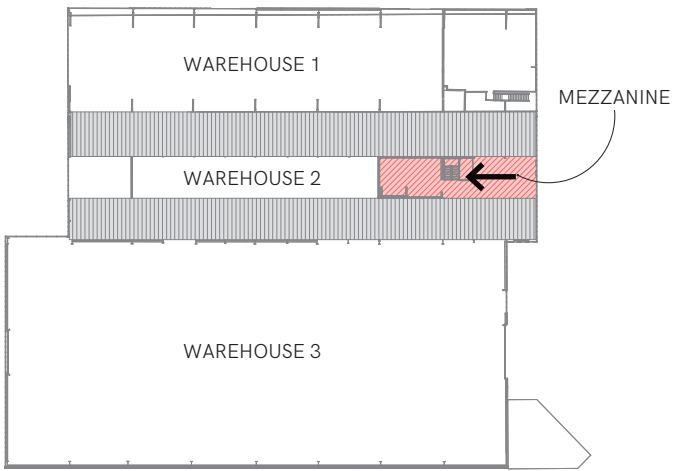


Existing Warehouse 3





Existing Mezzanine





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Project Brief



# 3.1 Brief

**Project Objectives.**

Regeneration of existing under-utilised and undervalued site/ building fabric of the former AG Barr factory through the:

- Establishment of Scotland’s Centre of Excellence for Canals & Traditional Skills in partnership with HES to promote learning and engagement and improve qualification levels. The Centre would be a step change in how traditional skills training would be delivered in Scotland and have a multi-generational impact
- Provision of modern, improved operational and maintenance facilities for SC, thereby driving operational savings and enabling more efficient and innovative SC working practices.
- Contribute to the SC, HES and broader government targets for Net Zero.

**Accommodation Brief.**

SC and HES facilities including:

- Public facing entrance and facilities (toilets/showers)
- Office (up to 30 desks)
- Classrooms/training spaces
- Meeting/breakout spaces
- Building support functions (comms rooms/first aid/plant)

Dedicated workshop space for SC and HES including:

- Dedicated SC and HES welfare facilities for up to 60 staff student/volunteers
- SC and HES direct workshop entrances
- Workshop linked HES classroom



Union Inn from the Forth and Clyde Canal



### 3.2 Existing SC an HES Accommodation

SC and HES currently operate from existing facilities that, while functional, are increasingly constrained in terms of space. These current workshop environments are insufficient to meet the growing demands of their daily operations, particularly in the areas of training, skill development, and restoration work.

The proposed Centre of Excellence for Canals & Traditional Skills aims to address this limitation by providing a significantly larger and more adaptable space. This expanded facility will not only accommodate the increasing operational needs of both organisations but also foster opportunities for skill enhancement, knowledge transfer, and the preservation of traditional craftsmanship. New training programs and meeting spaces provided at the larger facility will allow the building to serve as a hub for upskilling whilst ensuring the continuation of Scotland’s heritage.



Scottish Canals existing facilities at West Mains



Historic Environment Scotland existing facilities at Forth Bank

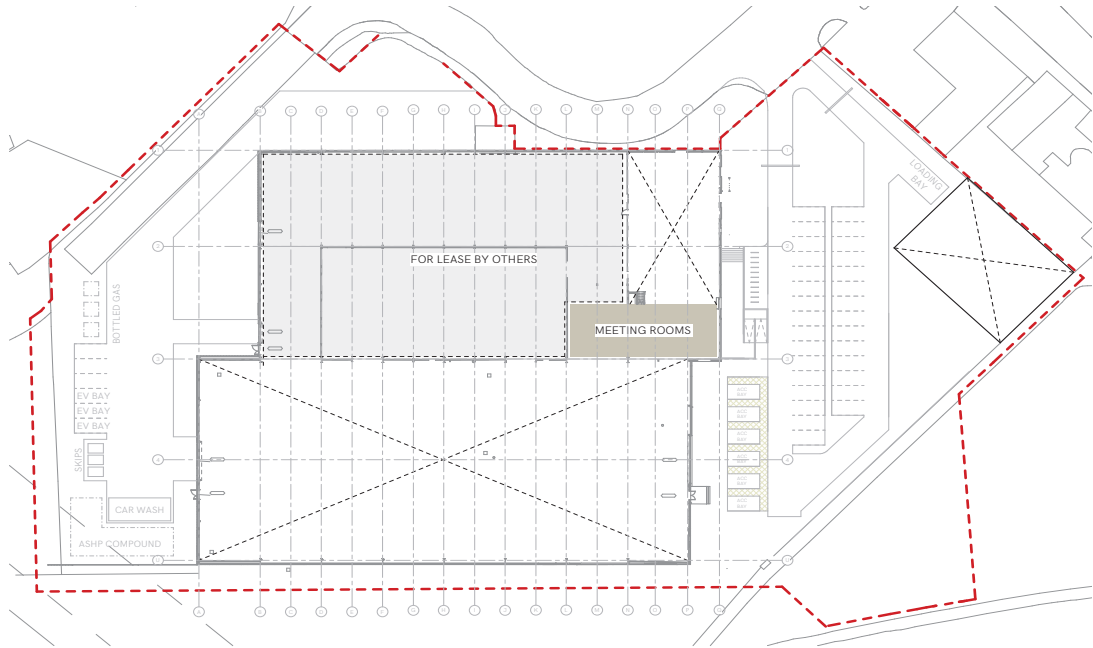


3.3 Feasibility

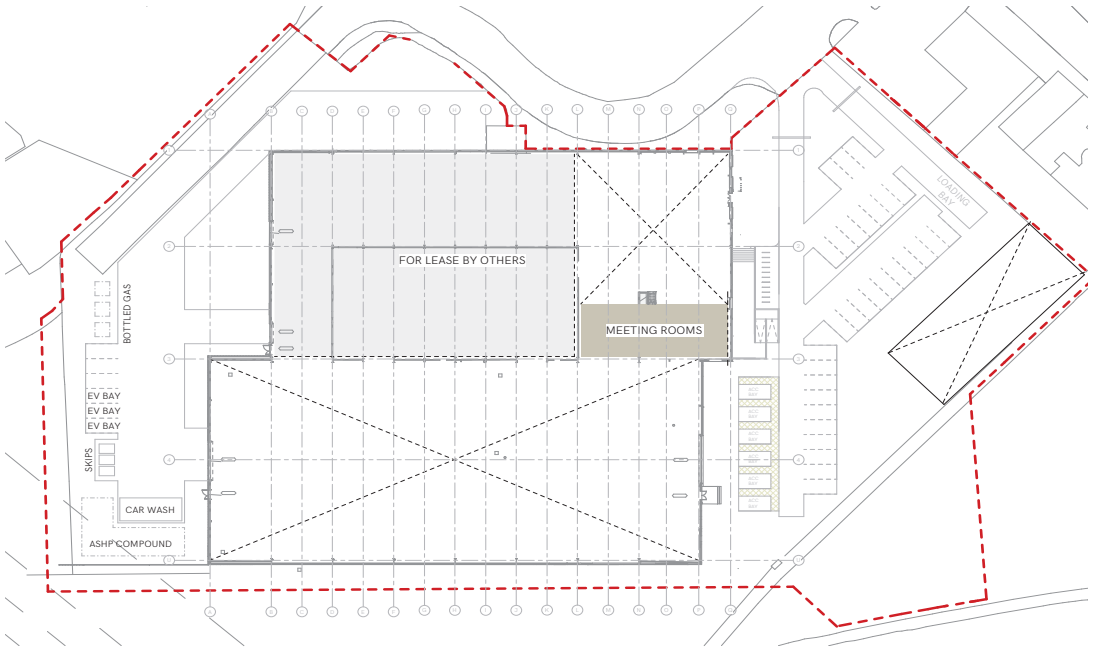
The design process began with a thorough feasibility assessment which included site analysis and an evaluation of the existing building. This analysis was conducted to inform the development of various floor plan options in accordance with SC requirements. Various designs were explored to assess the most effective layout, optimizing the existing arrangement and volume of spaces while considering the associated external spaces. Key factors such as noise control, sequencing and access played a key role in shaping the proposed options and ensuring a functional design that addressed both practical and aesthetic considerations.

Working alongside SC, HES and Robertson Construction we have now developed a proposal to refurbish and repurpose the existing industrial warehouse. The design brings various elements together approved by the client and aligned with the Project Brief for Lock 16 - Scotland's Centre of Excellence for Canals & Traditional Skills.

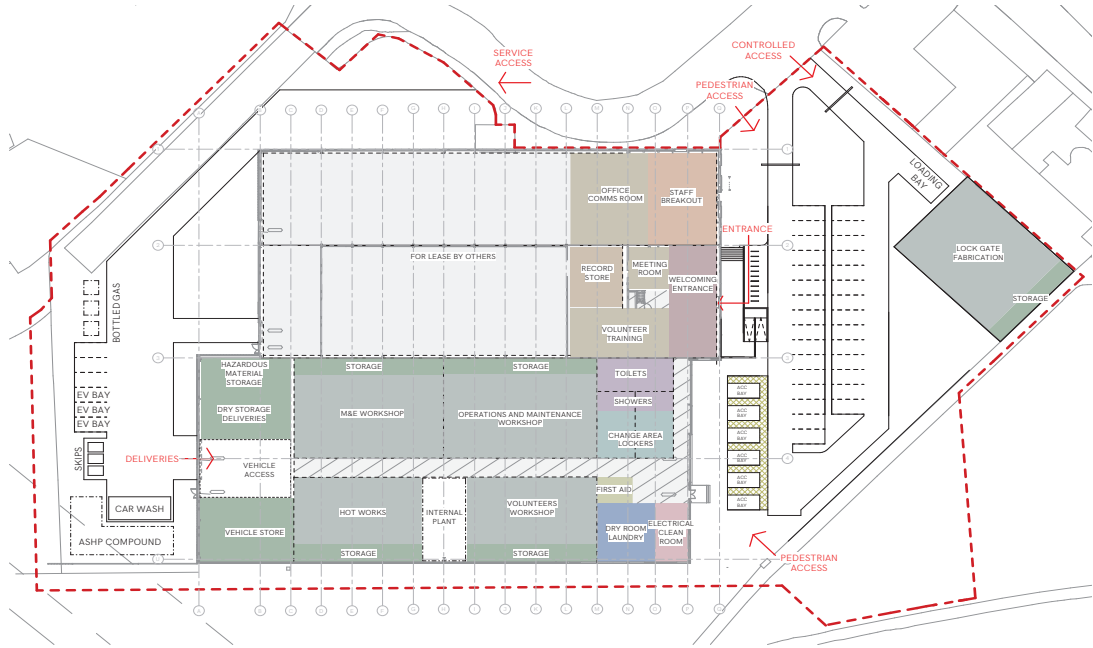
Throughout the development process we have discussed thoroughly how the existing structure can be adapted to meet various requirements and perform various functions. The outcome enhances the user experience, supports the local community and visitors with new amenities and employment opportunities and by physically improving the permeability of the site.



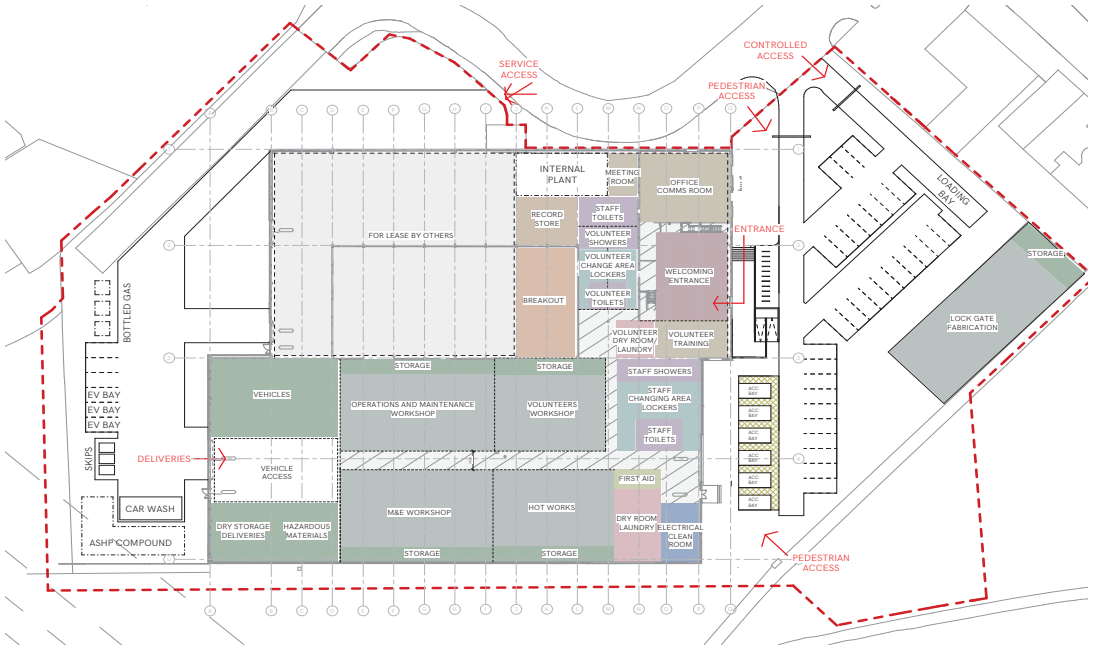
Option 01 - Mezzanine Level



Option 02 - Mezzanine Level



Option 01 - Level 00



Option 02 - Level 00



### 3.4 Design Development

Design Development Summary:RIBA Stage 1 to 2 -

The design development process reflects the progression from RIBA Stage 1 (Preparation and Brief) to RIBA Stage 2 (Concept Design). At Stage 1, the focus was on defining the project brief, outlining high-level spatial requirements, and setting initial objectives. By Stage 2, the design was further developed to provide a coordinated concept that responds to stakeholder input, including structural, servicing, and spatial considerations.

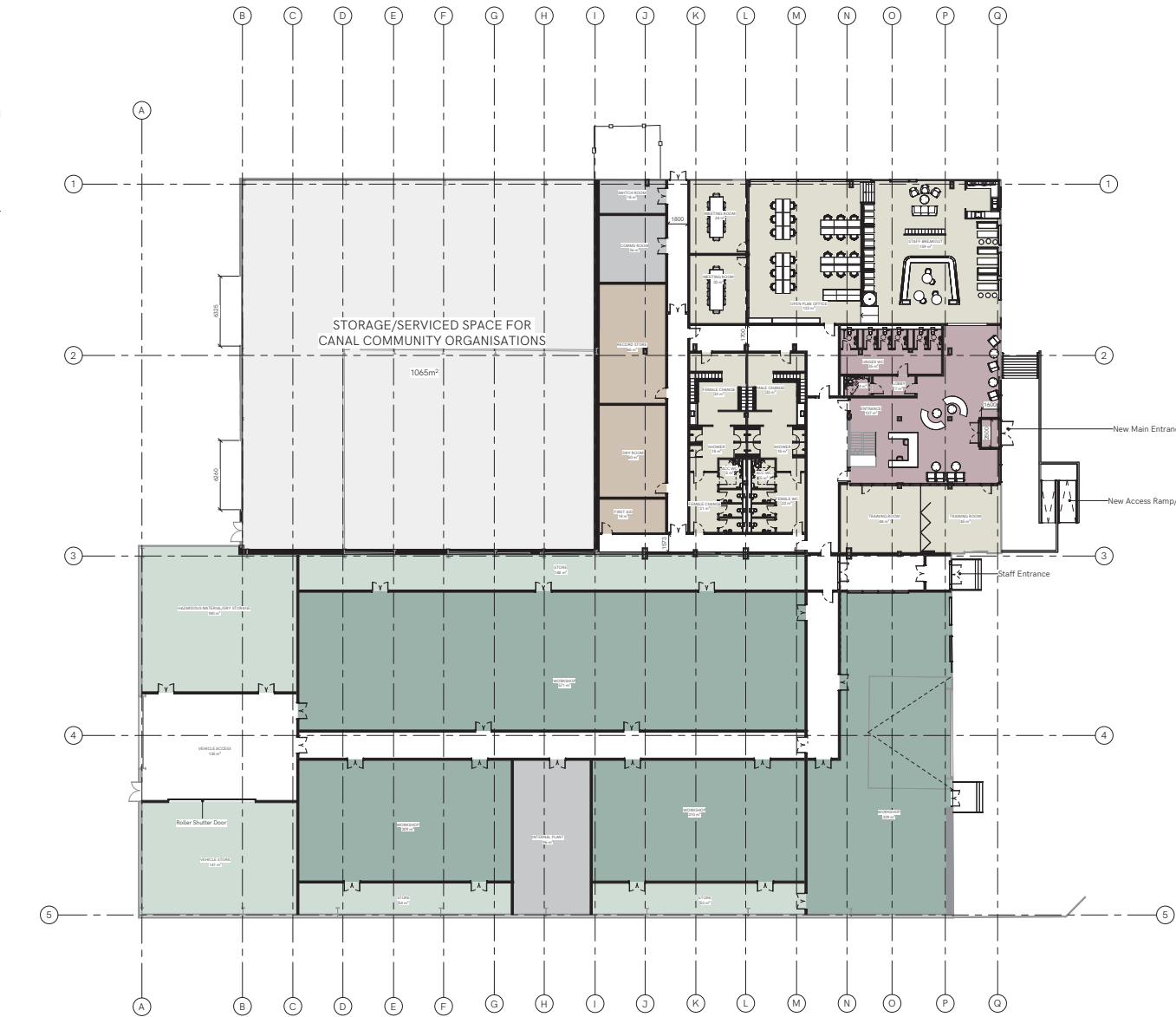
Following the inclusion of HES as a project partner, the Stage 1 brief and layouts were revisited to incorporate their specific requirements. This led to the following key changes:

Workshop Spaces:

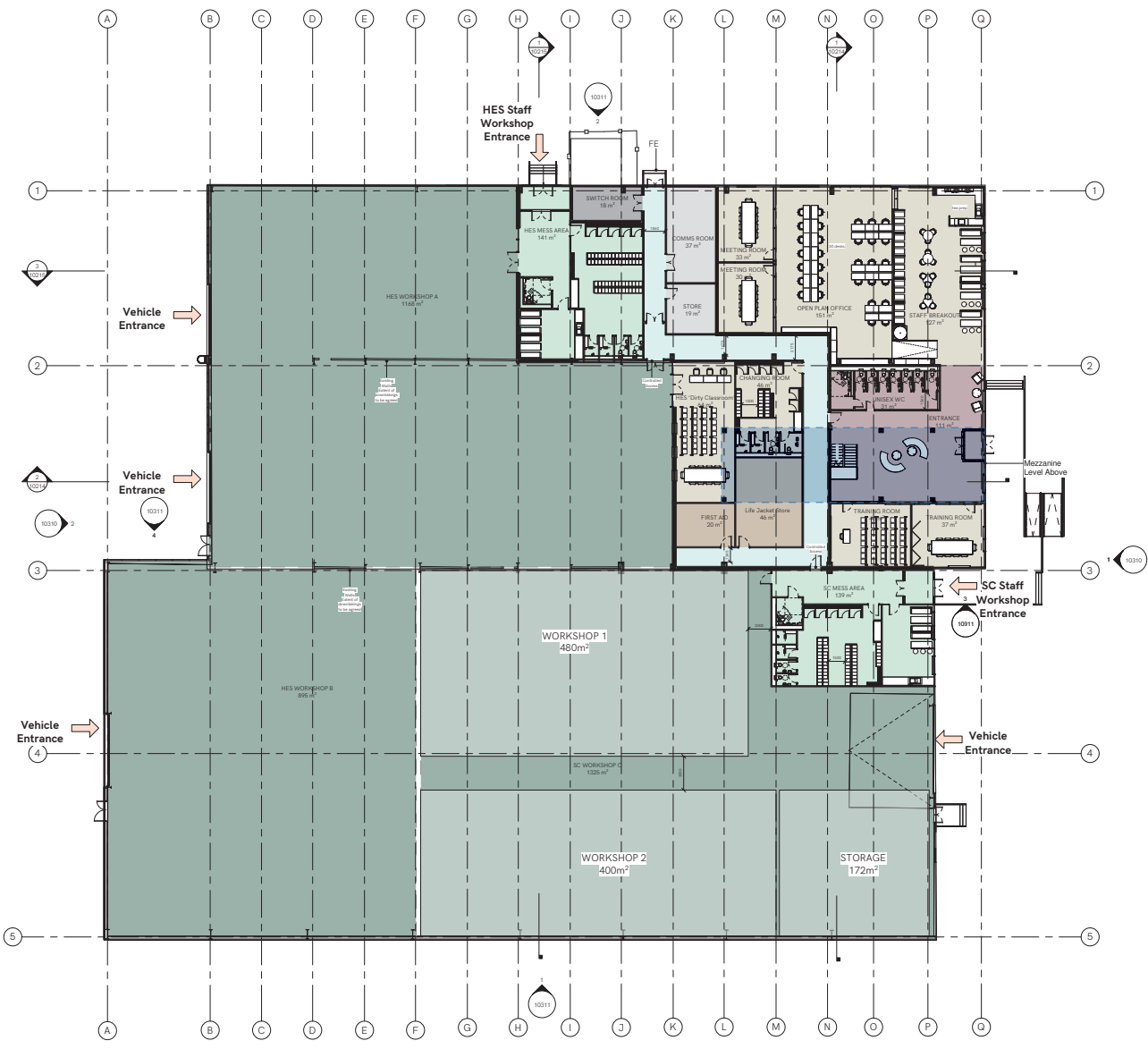
- Workshop requirements were re-scoped to address the specific needs of both HES and SC .
- Dedicated workshop staff welfare facilities were introduced, including toilets, showers, and changing areas.
- Separate, dedicated workshop entrances were added for both HES and SC to enhance functionality and access.

Shared Facilities:

- Office and breakout spaces were reconfigured to function as shared facilities for HES and SC staff, fostering collaboration and efficiency.
- Public-facing facilities were reduced to account for the newly incorporated dedicated workshop facilities for HES and SC staff.



RIBA Stage 1 Layout



RIBA Stage 2 Layout



Design Development Summary:RIBA Stage 2 to 3

The design development process reflects the progression from RIBA Stage 2 (Concept Design) to RIBA Stage 3 (Developed Design). At Stage 2, the focus was on refining the project brief into a coordinated concept that incorporated stakeholder input, high-level spatial planning, and preliminary structural and servicing strategies. By Stage 3, the design was further detailed, with greater emphasis on technical coordination, regulatory compliance, and operational functionality.

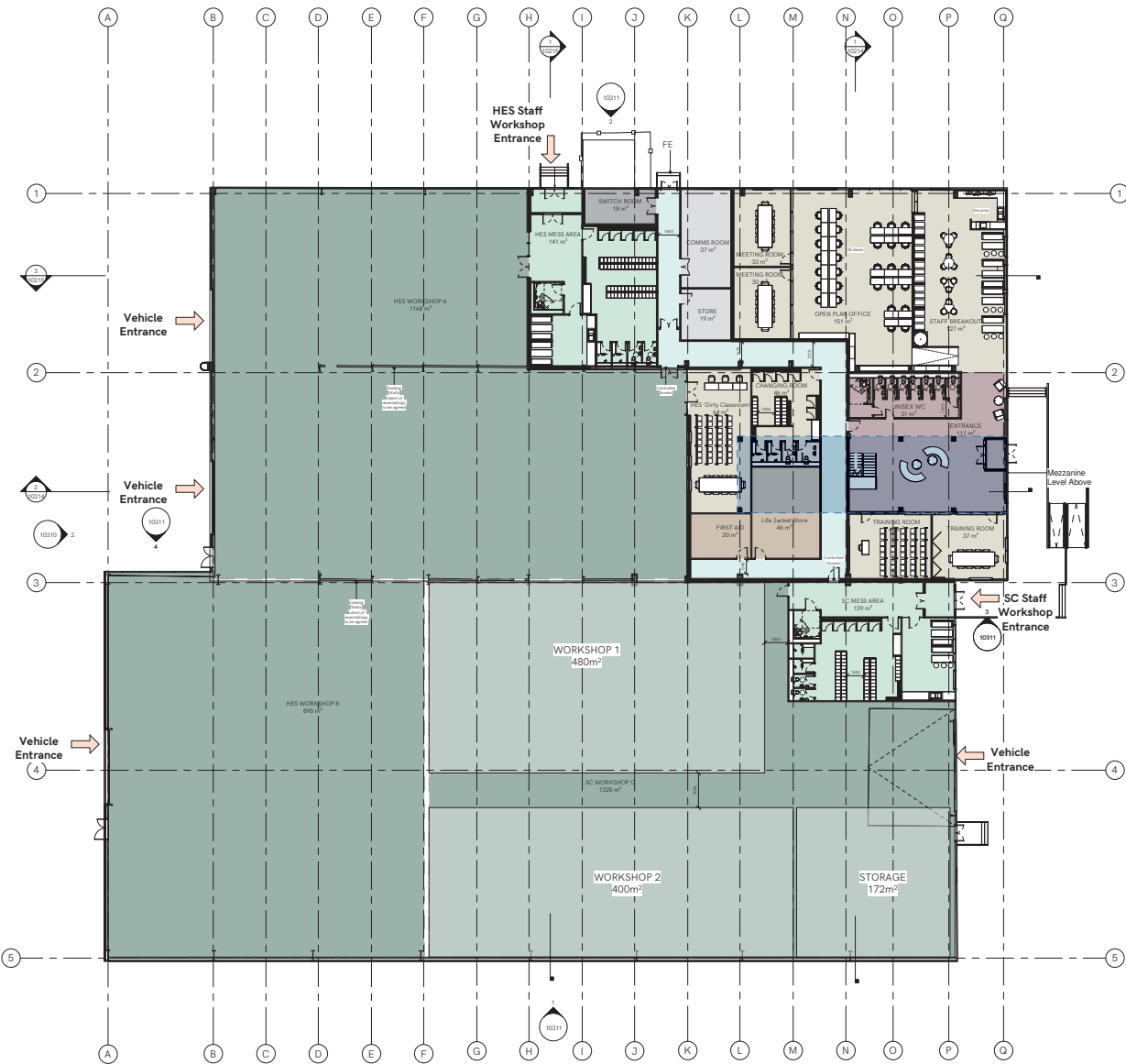
The integration of stakeholder feedback and a focus on operational requirements resulted in the following key changes:

Workshop Spaces:

- Workshop layouts were further refined, with spaces subdivided into distinct zones to accommodate specific operational needs for both HES and SC.
- Dedicated welfare facilities associated to HES and SC workshop staff were developed based on orgainsational requirement.

Shared Facilities:

- Office and breakout areas were further developed into shared spaces for HES and SC, additional smaller meeting spaces were introduced.
- Public-facing facilities were reviewed and revised to include just toilet provision.



RIBA Stage 2 Layout



RIBA Stage 3 Layout



### Value Engineering Outcome: Removal of Mezzanine

To compensate for the loss of the mezzanine level, two additional training spaces have been added at ground floor level in RIBA Stage 4. Further changes include a revised layout of the facilities for HES and an update to the layout of sanitary facilities.





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Design Approach



KEY.

1. Section of existing fencing to be replaced.

2. New pedestrian access to be provided.

3. Existing building entrance to be relocated with new access stairs/ramp.

4. The external fabric of the building to be upgraded internally to comply with current standards and to significantly improve building aesthetic appearance.

5. Existing extension in the North-East corner to be demolished.
6. Existing garage building to be demolished.

7. New car park to be provided.

8. South Yard to be utilised

9. Public facade to be upgraded with new glazing to improve daylight internally and to create a more welcoming approach externally

10. New access stair and ramp to the proposed main entrance to address changes between external and internal levels.

11. EV Charging Spaces

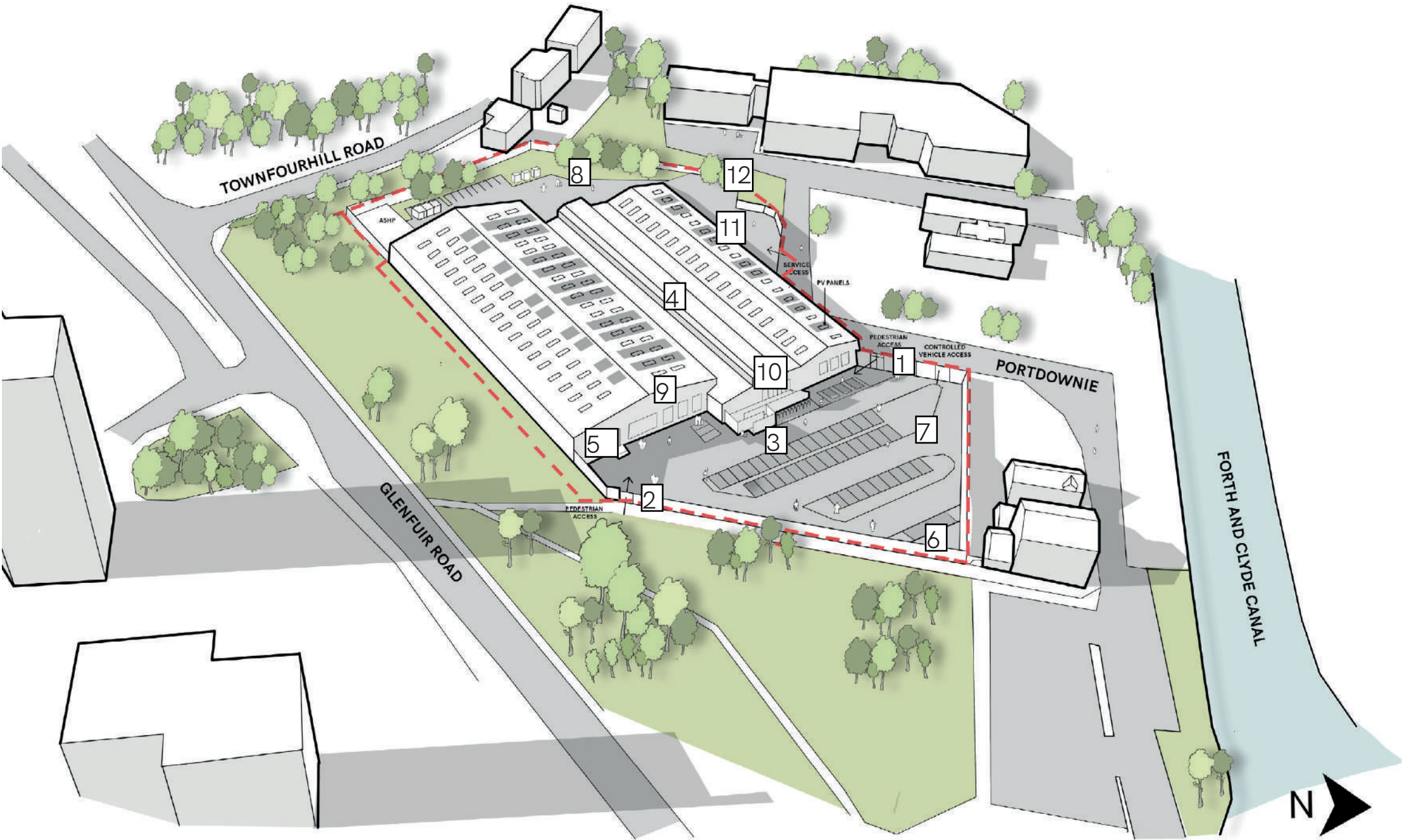
12. New boundary treatment to replace temporary herras fencing.

4.1 Proposed External Works

Whilst much of the existing site will be retained the proposed changes are expected to improve the public facing appearance

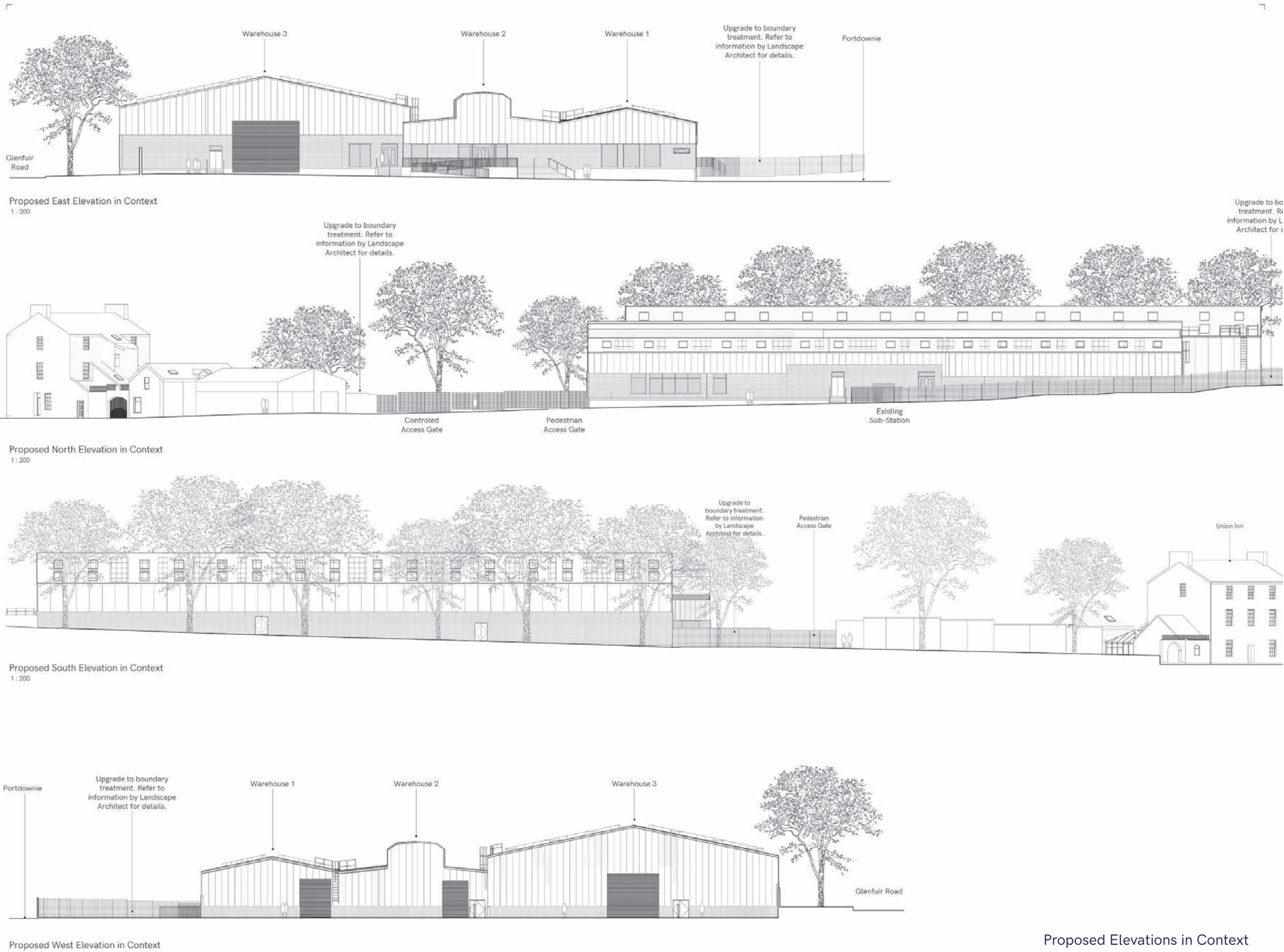
The proposed landscaping strategy will provide an inviting approach to the new main entrance of the building meanwhile modifications to the existing North and East facades will reduce the sense of privacy with the intorduction of new glazing. New access and exit routes will be provided to the South and North to suit various building users and overall safety and security will be improved with upgrades to elements of the site boundary.

The landscaping plan also incorporates measures to support biodiversity, and transport considerations are addressed in the transport statement. Visitors will be able to access the site by bike, bus, car or on foot. Electric vehicle charging will also be available on site for corporate fleet vehicles.



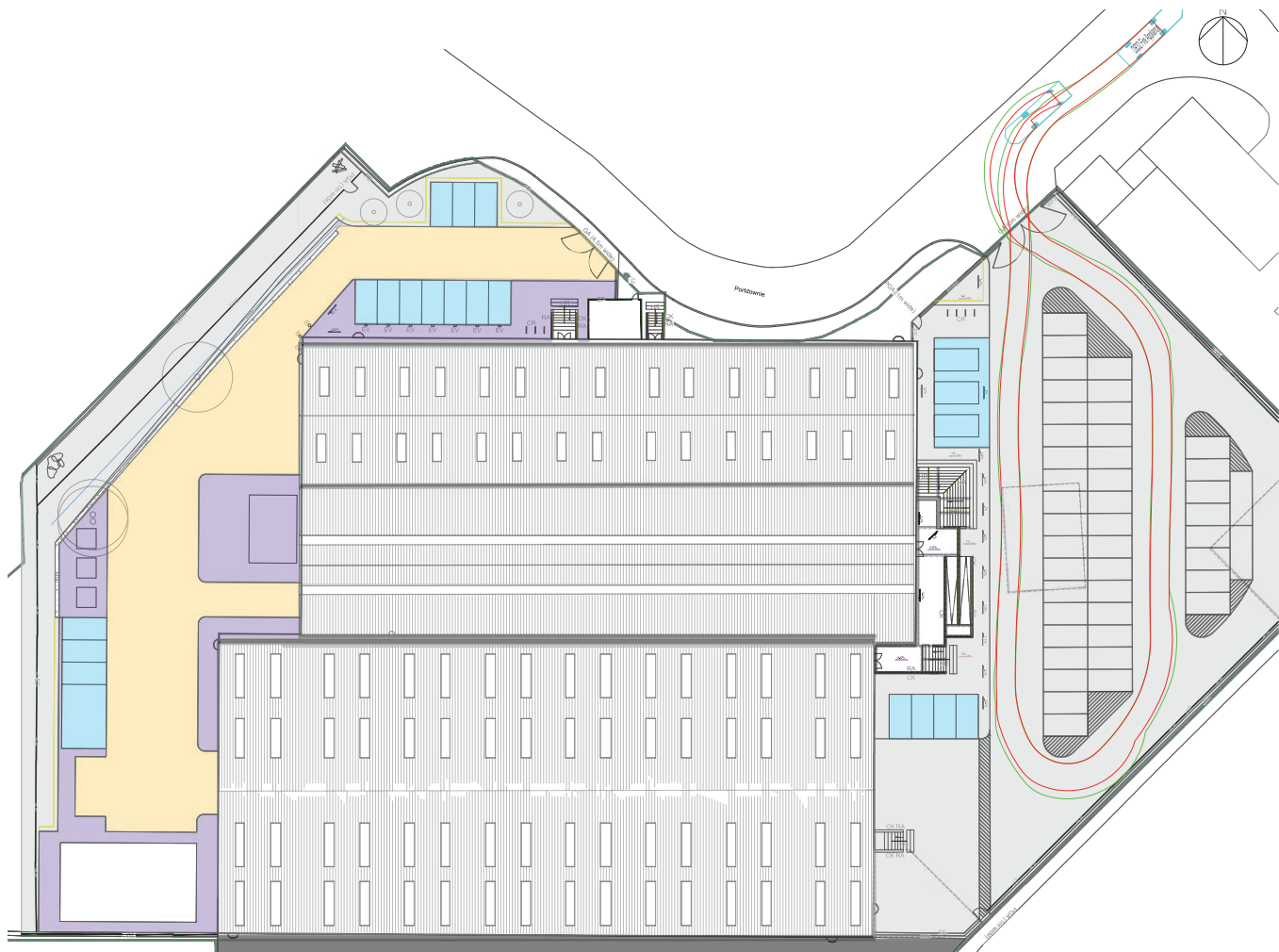


4.2 Proposed Design in Context



Proposed Elevations in Context

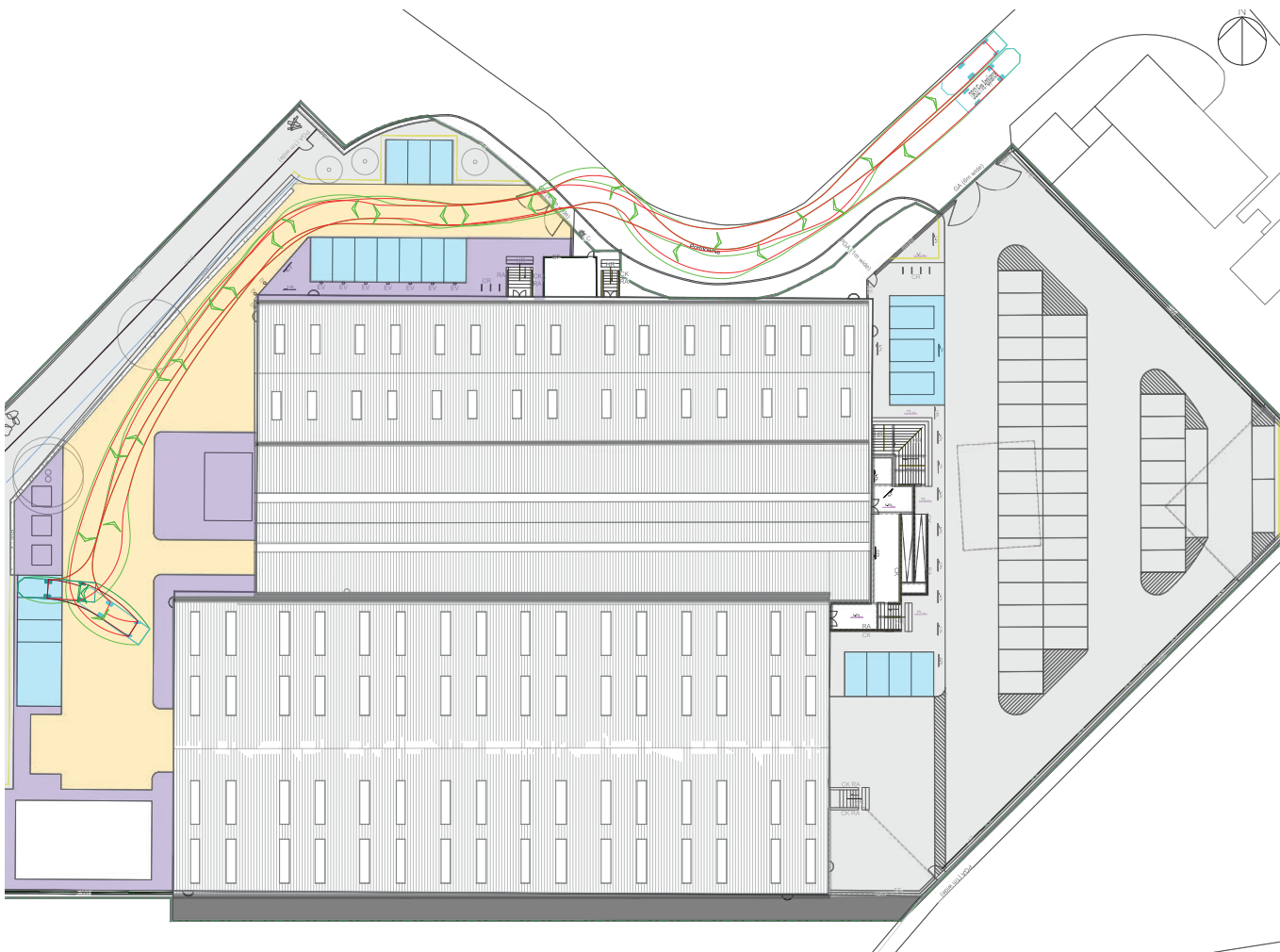
4.3 Proposed Site Access



Sweep Path Analysis for Fire Engine Vehicle - North Yard

The North Yard car parking layout has been carefully designed to accommodate access for Scottish Fire and Rescue services, ensuring that fire engines can reach the building safely. The design allows for sufficient space for the vehicle to turn around and exit without requiring the use of any designated parking spaces. This ensures that emergency access remains unobstructed at all times.

The car park design also allows for large vehicles to service the building facilitating SC workshop deliveries



Sweep Path Analysis for Fire Engine Vehicle - South Yard

The design of the South Yard has also been carefully planned to ensure unrestricted access for Scottish Fire and Rescue services. It provides ample space for a fire engine to safely reverse, turn around, and exit the area without obstruction. This thought out approach to the external layout guarantees that all sides of the building remain fully accessible, enabling emergency services to enter the building from any direction in the event of an emergency.

The design of the rear yard also facilitate large vehicle deliveries to the HES workshop



#### 4.4 Proposed Landscaping

The external landscape associated with the scheme can be divided into two key areas: north yard and south yard. Both spaces serve as functional working areas or parking areas with spaces for amenity and enhancements.

The north yard will be accessed via vehicle and pedestrian gates from Portdownie Street and an additional pedestrian gate is proposed on the East boundary to provide linkage with an adjacent open greenspace. The principle function of this space is parking provision and access to the building. It is proposed that the majority of the north yard will be left as an asphalt surface and repaired as necessary, with the addition of PCC block paving to pedestrian areas at the main entry points to the building. Due to internal building levels, a raised platform at the main building entrance will be constructed with steps and ramps to ease access. The platform will be constructed from brick matching the building materials and the level difference will be addressed with mild steel horizontal railings.

The north yard boundary will be secured with a mixture of existing and new fencing in the form of weld mesh fencing.

The yard will be enhanced with two areas of soft landscape including meadow seeding to increase site biodiversity and to minimise the extent of hard landscape.





The south yard function is for servicing, access, and storage but also Electric Vehicle charging parking. It is proposed that the majority of the yard will be surfaced with permeable reinforced aggregate surfacing to vehicle movement areas and with asphalt for storage areas/pedestrian movement zones. The parking areas will be surfaced with permeable pcc block to form part of the drainage strategy.

This space also contains a large area of soft landscape associated with the embankment and former road located along the west boundary of the site. It is proposed that this area is reprofiled to help improve site access and overseeded with meadow seeding of Scottish origin to boost local biodiversity, and to mitigate for the loss of existing scrub vegetation. To encourage nature on site it is proposed to locate log & stone plies within the meadow to help the invertebrate population and bird & bat boxes on building elevations.

The majority of existing fencing and boundary walling that encloses the south yard will be retained, with the exception of fencing along Portdownie Street which will be replaced with a weld mesh system.

The loss of existing scrub trees will be mitigated with the addition of native species like alder, birch, and rowan trees as shown on the LUC planting proposal.

Cycle parking arrangements will be provided for both spaces at key locations, totalling 14 parking slots for the site.

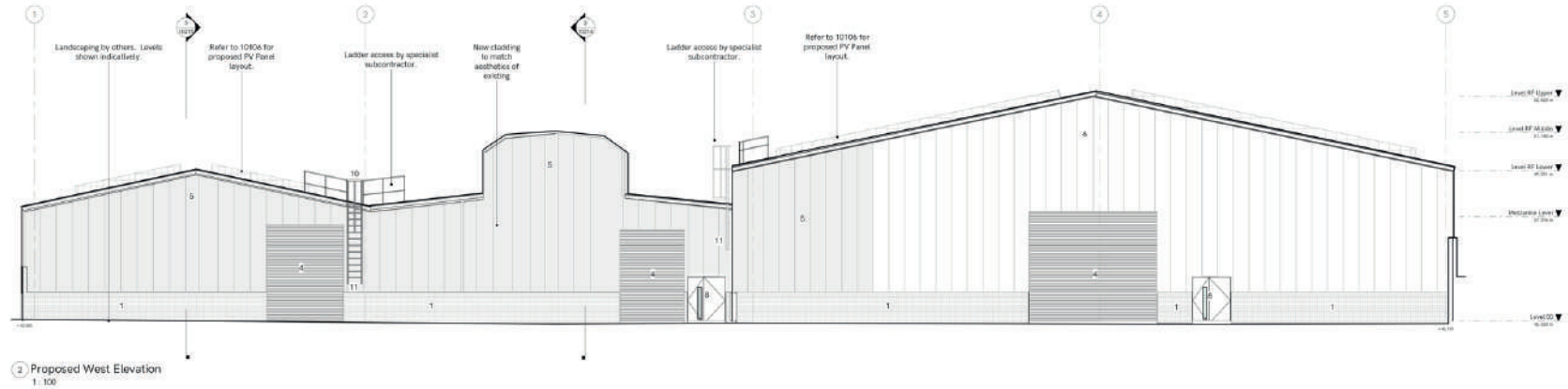
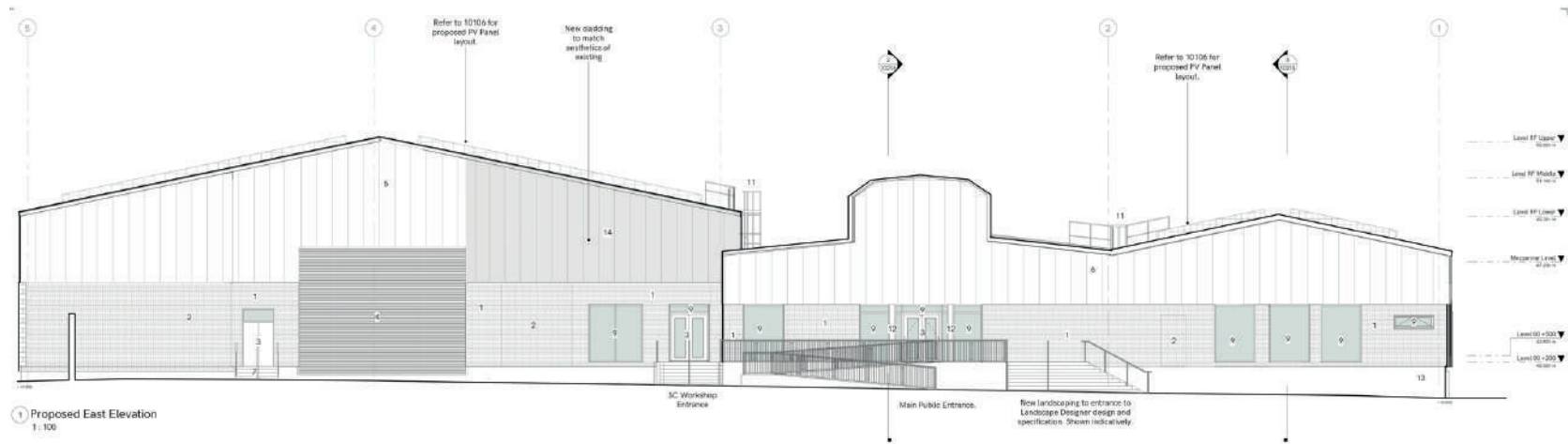




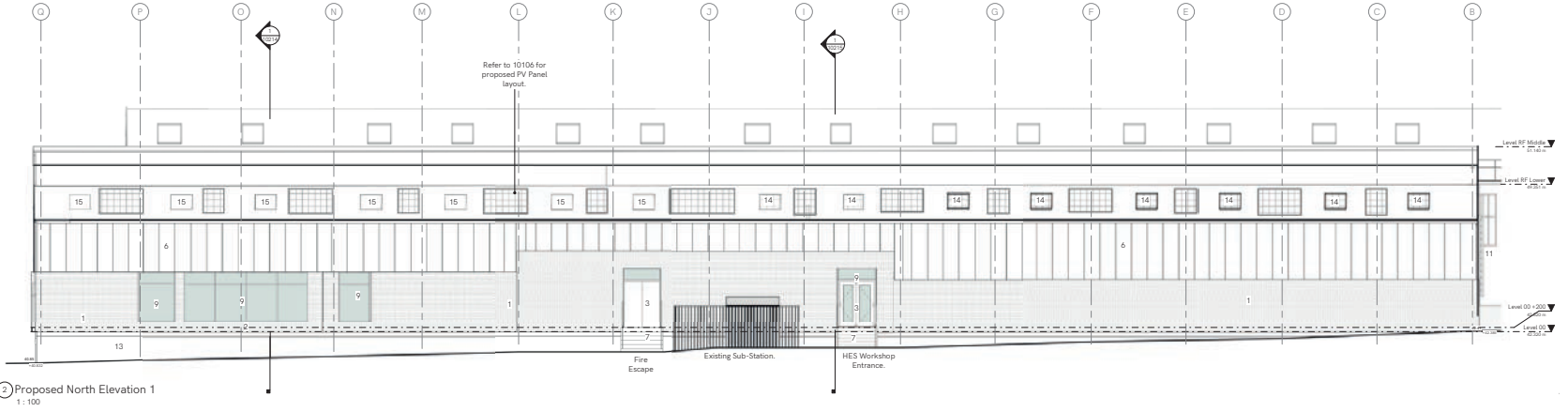
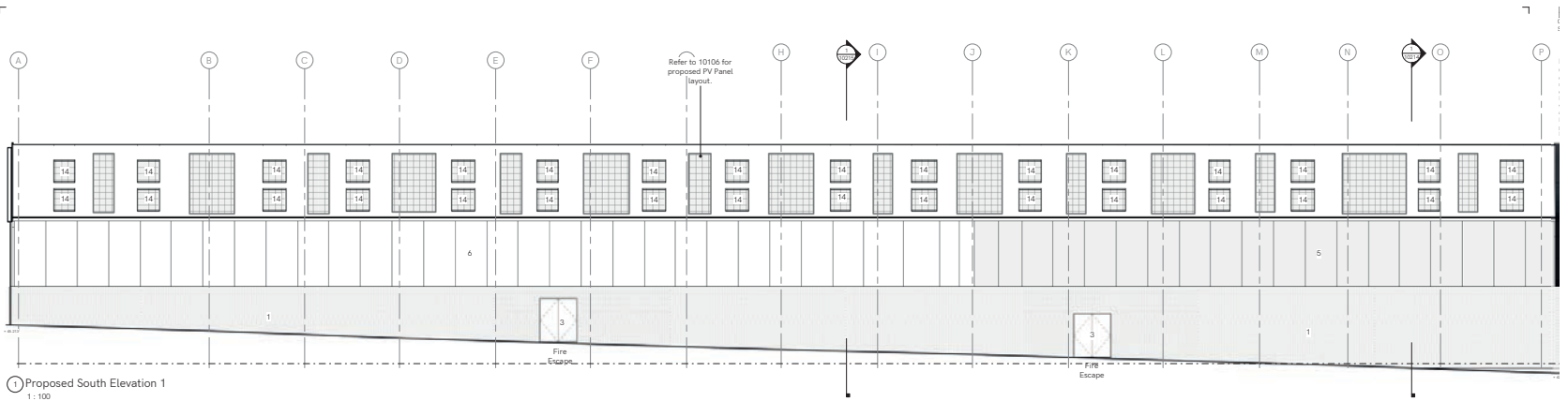
# 4.5 Proposed External Appearance

The existing building’s aesthetic is distinctly industrial, reflective of its original use. To adapt this style for its new purpose, the North and East facades will be enhanced with the addition of new glazing. This amendment will create a visual connection between the interior and exterior, suggesting a more open, welcoming space behind the facade. The overall appearance of the building will also be improved, with cleaning, tidying, and any necessary repairs undertaken to restore its condition, ensuring a cohesive and inviting presentation.

For maintenance purposes a new roof access system will be installed as part of the proposed enabling works. The design of the system is considerate of site security and safety when cleaning gutters and maintaining the proposed photovoltaic (PV) panels.



- KEY:**
- 1. Existing Brickwork: cleaned.
  - 2. New brickwork to match existing. To be primed and painted. Colour: RAL 7016.
  - 3. New external door. Colour: RAL 7016.
  - 4. Existing roller shutter door.
  - 5. Existing polycarbonate wall cladding.
  - 6. Existing Trapezoidal plastic coated steel cladding panels - Kingspan KS1000 RW
  - 7. New external stair with associated handrails.
  - 8. Existing door.
  - 9. New glazing / curtain walling. Frame colour: RAL 7016.
  - 10. New barrier.
  - 11. New galvanised ladder between roof and ground level, with safety cage and step over platform. (Bottom of ladder to be retractable and lockable)
  - 12. Aluminium flashing. Colour to match curtain wall framing: RAL 7016.
  - 13. Existing slab: cleaned
  - 14. New cladding to match aesthetics of existing Kingspan KS1000 RW

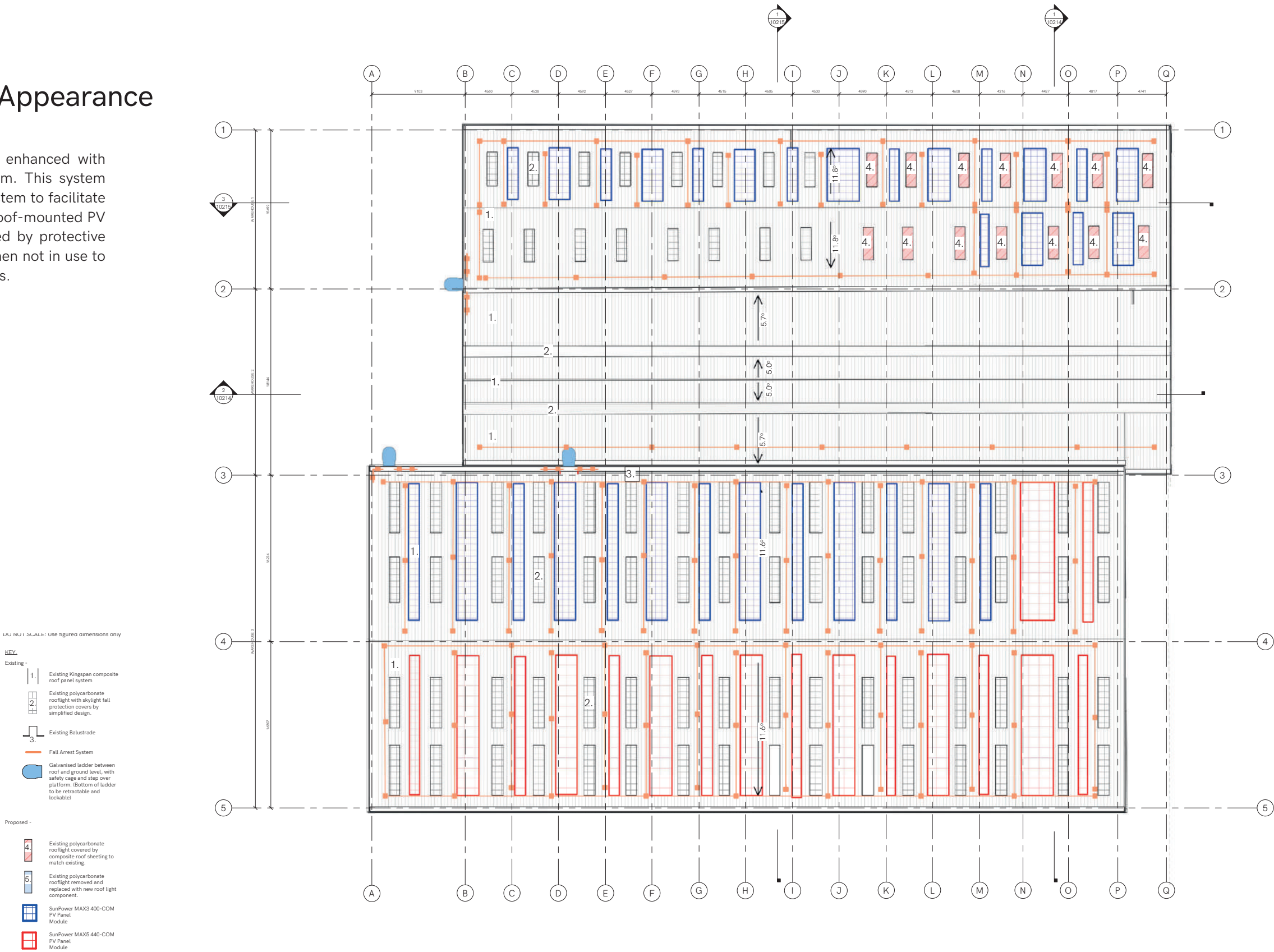


- KEY:**
- 1. Existing Brickwork to be cleaned
  - 2. New brickwork to match existing
  - 3. New external door. Colour: RAL 7016
  - 4. Existing roller shutter door.
  - 5. Existing polycarbonate wall cladding
  - 6. Existing Trapezoidal plastic coated steel cladding panels - Kingspan KS1000 RW
  - 7. New external stair with associated handrails.
  - 8. Existing door.
  - 9. New glazing / curtain walling. Frame colour: RAL 7016.
  - 10. New roof walkway system
  - 11. New galvanised ladder between roof and ground level, with safety cage and step over platform. (Bottom of ladder to be retractable and lockable)
  - 12. Aluminium flashing. Colour to match curtain wall framing: RAL 7016.
  - 13. Existing slab to be cleaned.
  - 14. Existing Rooflights with skylight fall protection covers.
  - 15. Existing rooflights to be covered. Refer to 10106 for full details.
  - 16. New cladding to match aesthetics of existing Kingspan KS1000 RW



# 4.5 Proposed External Appearance

The building maintenance strategy will be enhanced with the installation of a new roof access system. This system incorporates new ladders and a fall arrest system to facilitate gutter cleaning and maintenance of the new roof-mounted PV panels. Existing rooflights will be safeguarded by protective cages, and ladders will be securely locked when not in use to ensure safety and prevent unauthorised access.



# 4.6 Proposed Internal Arrangement

The refurbished Lock 16 facility is proposed to include the following:

- Welcoming Entrance
- Staff Break Out Area
- Staff Offices
- Meeting Rooms
- Meeting Pods
- Training Rooms
- Staff Changing Rooms
- Unisex Toilets
- Accessible Toilets
- Staff showers
- SC Life Jacket Store
- First Aid Room
- HES 'Dirty Classroom'
- HES 'Mess' area including dining, changing rooms, accessible change and toilets.
- SC 'Mess' area including dining, changing rooms, accessible change, drying room and toilets.
- HES Workshop
- SC Workshop
- SC M&E 'Clean' Workshop
- Switch Room
- M&E Plant





# 4.7 Proposed Interior Design

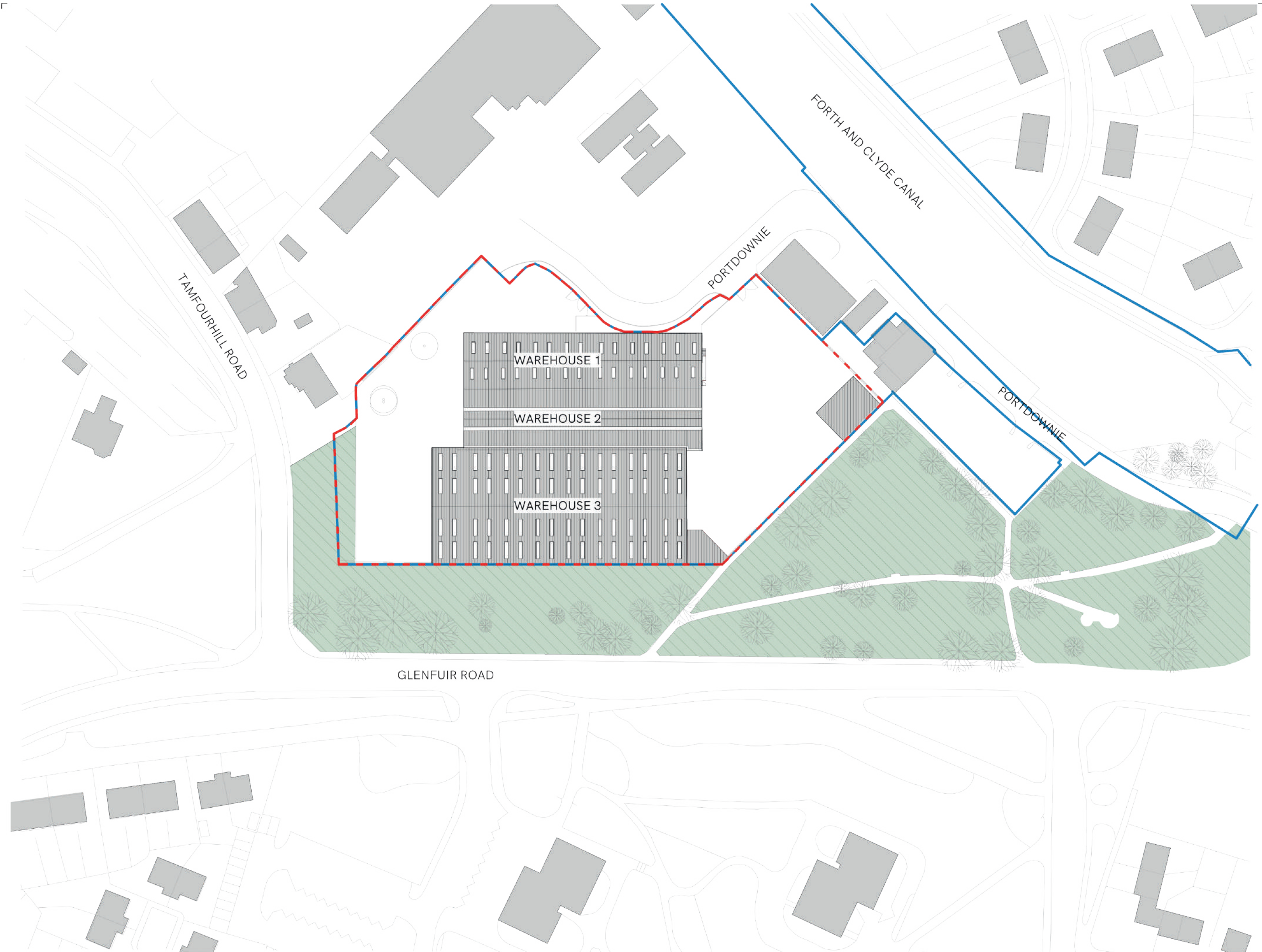
Whilst the existing industrial aesthetic is well suited to the proposed workshops, other areas require new finishes and furnishings. The internal colour palette will be suited to both SC and HES who will provide seprate furnishings appropriate to their own branding and preferences.

The proposal makes use of the existing building height by introducing a change of level from the staff breakout space to the open plan office. This elevation change offers an element of privacy to the office without isolating it. The raised floor level also opens up an opportunity for sustaiable benefits by creating space for additional under slab insulation.

# 5

## Key Architectural Drawings



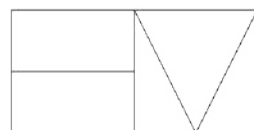


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**KEY:**  
— Land in Scottish Canals Ownership.  
- - - Existing Site boundary.

EXISTING SITE AREA: 9905.0m<sup>2</sup> / 0.99Ha

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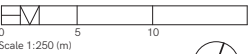
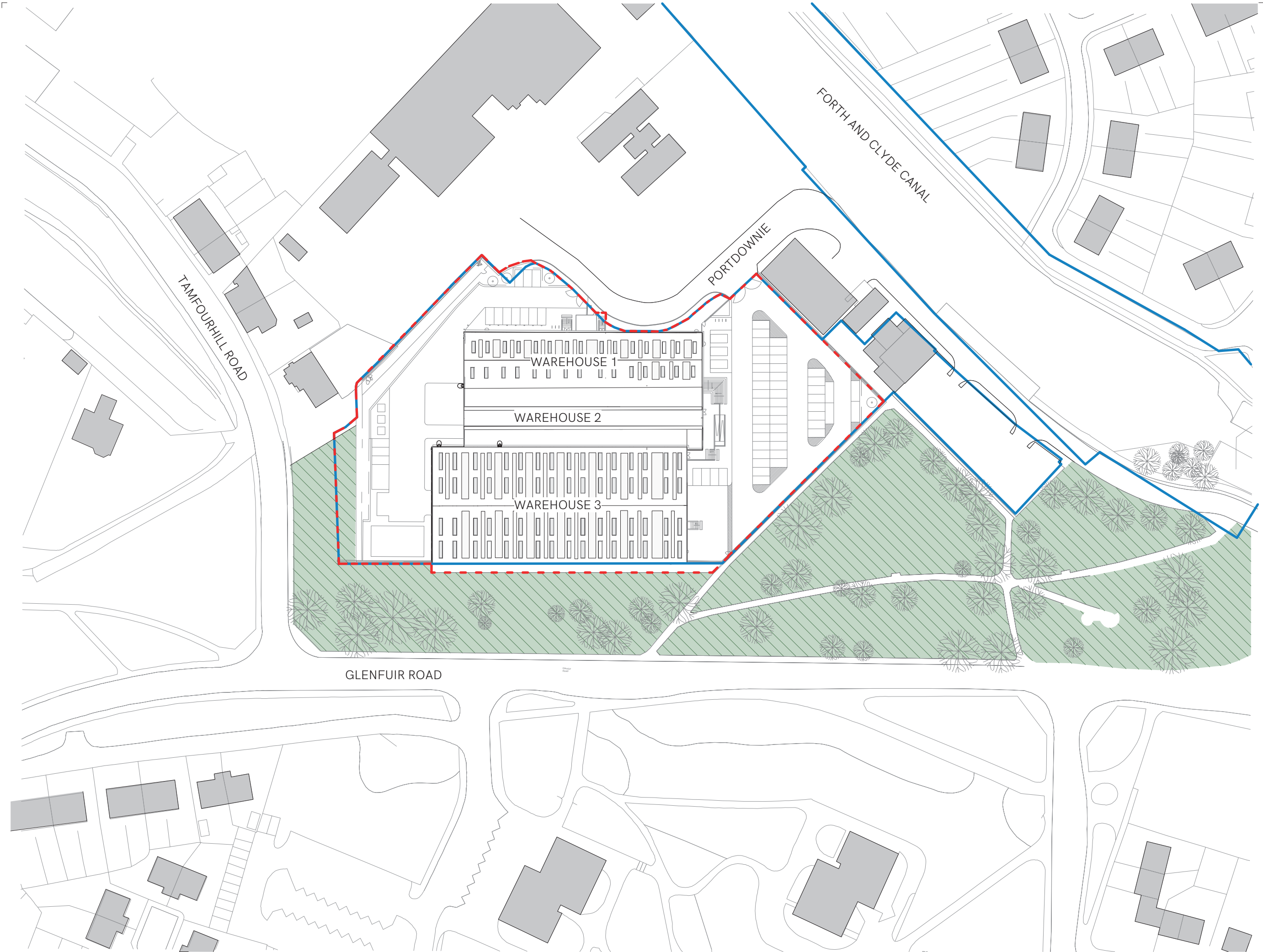
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Existing Location Plan

Scale @ A1	Checked	Revision
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**Drawing Number**  
LOCK16-HML-ZZ-ZZ-DR-A-00-10003

Note:  
Previous versions under drawing =  
5133-HML-ZZ-ZZ-D-A-10001  
5133-HML-ZZ-ZZ-DR-A-10003



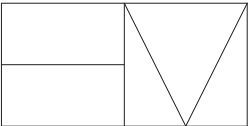


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**KEY.**  
— Land in Scottish Canals Ownership.  
- - - Proposed Site Boundary.

**EXISTING SITE AREA:** 9905.8m<sup>2</sup> / 0.99Ha  
**PROPOSED SITE AREA:** 10425m<sup>2</sup> / 1.04Ha

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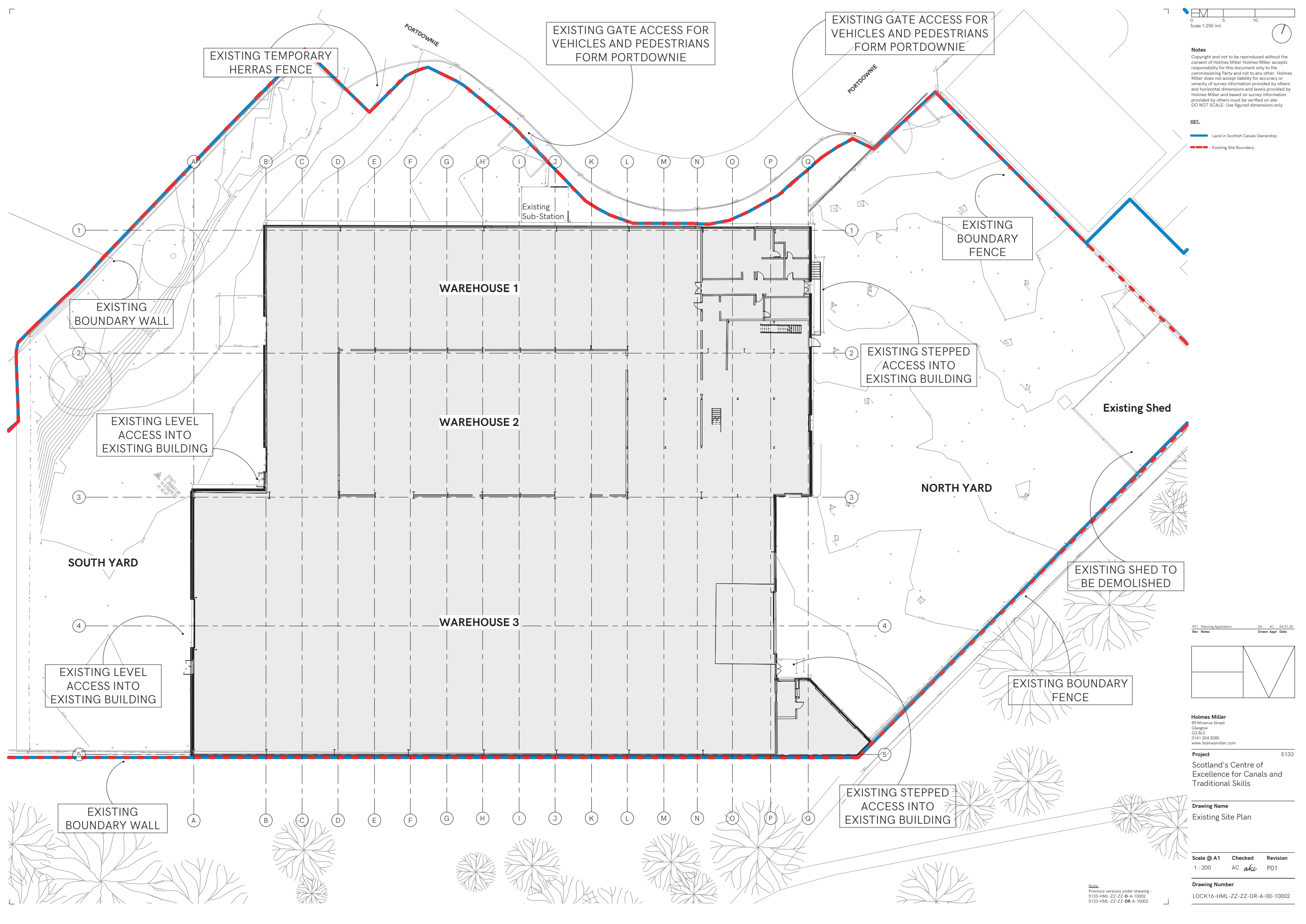
**Drawing Name**  
Proposed Location Plan

Scale @ A1	Checked	Revision
1 : 500	AC <i>ako</i>	P01

**Drawing Number**  
LOCK16-HML-ZZ-ZZ-DR-A-00-10101

Note:  
Previous versions under drawing -  
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5133-HML-ZZ-ZZ-DR-A-10101

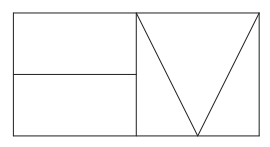




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**KEY**  
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Existing Site Boundary.

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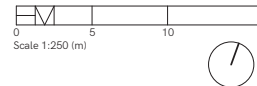
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**Revision** P01

**Drawing Number**  
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**Notes:**  
Previous versions under drawing:  
5133-HML-ZZ-ZZ-D-A-10002  
5133-HML-ZZ-ZZ-DR-A-10002

**NOTE:** TO BE READ IN  
CONJUNCTION WITH DRAWINGS  
BY LANDSCAPE ARCHITECT.



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**KEY.**

1. New Secure Fence
2. New Pedestrian Access
3. New Vehicle Access
4. New Entrance Access
5. New Bicycle Parking
6. New Car Park
7. New ASHP
8. Existing Sub Station
9. EV Bays
10. ACC Bays
11. Clear Area
12. HES External Racking
13. Skips
14. New low level fence with gate
15. New Stairs

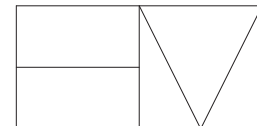


Planning Boundary

Land in Scottish Canals  
ownership outwith  
planning boundary



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Drawing Name

## Proposed Site Plan

Scale @ A1	Checked	Revision
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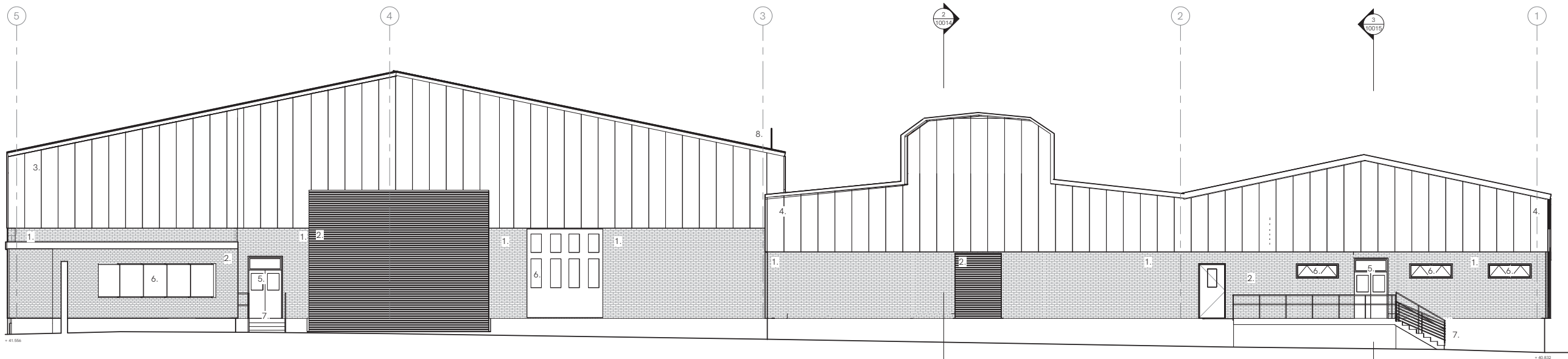
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Drawing Number

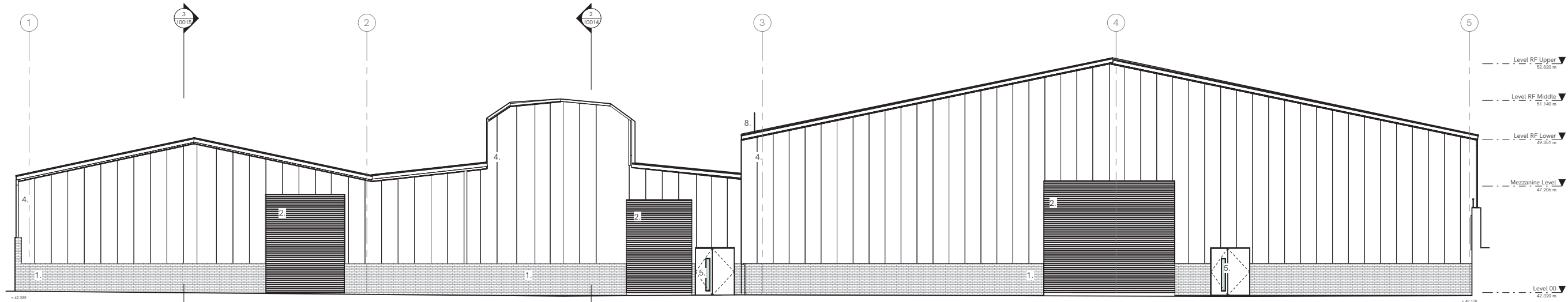
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Note:  
Previous versions under drawing -  
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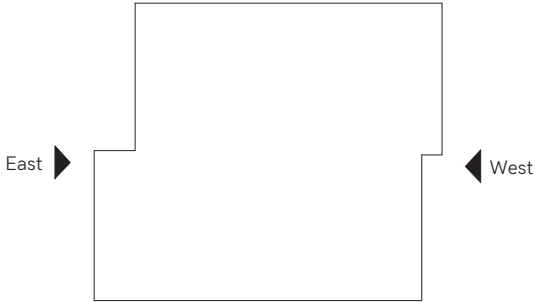




Existing East Elevation.  
1 : 100



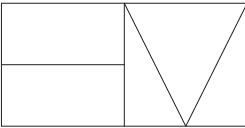
Existing West Elevation.  
1 : 100



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1. Existing Brickwork
2. Existing roller shutter door.
3. Existing polycarbonate wall cladding.
4. Existing Trapezoidal plastic coated steel cladding panels - Kingspan KS1000 RW
5. Existing door
6. Existing Window/Opening
7. Existing External Stair
8. Existing Balustrade

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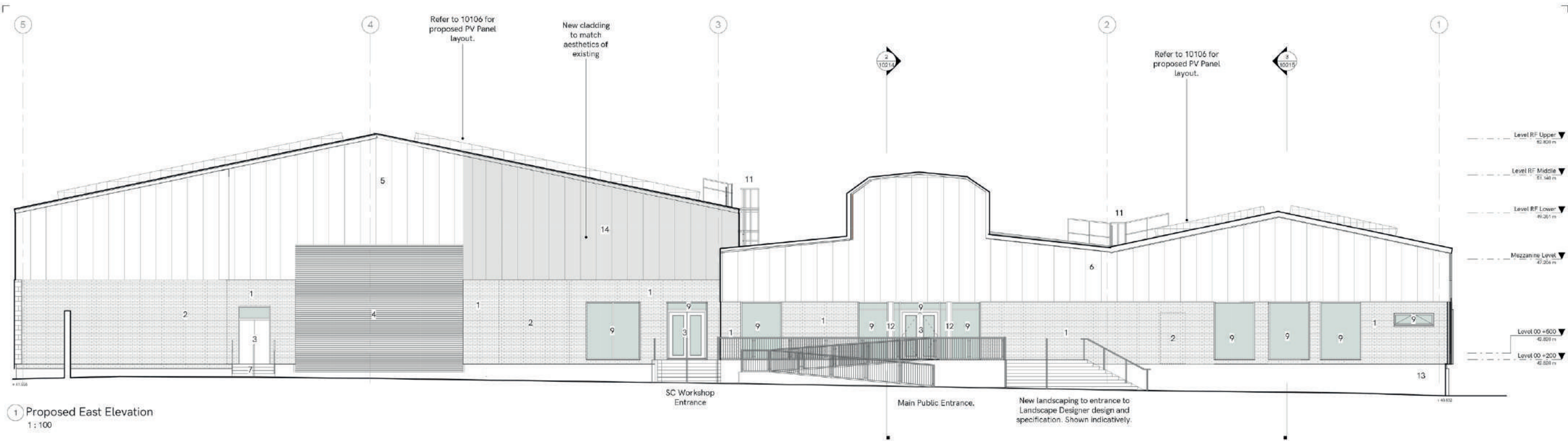
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**Drawing Name**  
Existing Elevations - East &  
West

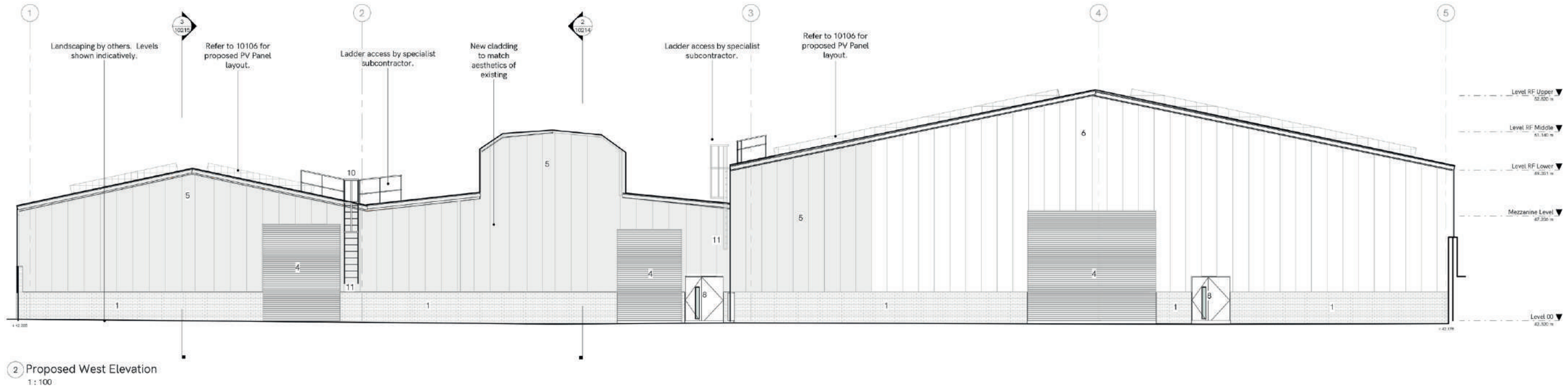
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**Revision** P01

**Drawing Number**  
LOCK16-HML-ZZ-ZZ-DR-A-00-10010

**Note:**  
Previous versions under drawing -  
5133-HML-ZZ-ZZ-D-A-10010  
5133-HML-ZZ-ZZ-DR-A-10010



1 Proposed East Elevation  
1:100



2 Proposed West Elevation  
1:100



Note: Previous versions under drawing  
5133-HML-ZZ-ZZ-D-A-10310  
5133-HML-ZZ-ZZ-DR-A-10310



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- KEY:**
- Existing brickwork: cleaned.
  - New brickwork to match existing. To be primed and painted. Colour: RAL 7016.
  - New external door. Colour: RAL 7016.
  - Existing roller shutter door.
  - Existing polycarbonate wall cladding.
  - Existing Trapezoidal plastic coated steel cladding panels - Kingspan KS1000 RW.
  - New external stair with associated handrails.
  - Existing door.
  - New glazing / curtain walling.
  - Frame colour: RAL 7016.
  - New barrier.
  - New galvanised ladder between roof and ground level, with safety cage and step over platform. Bottom of ladder to be retractable and lockable.
  - Aluminium flashing. Colour to match curtain wall framing: RAL 7016.
  - Existing slab: cleaned.
  - New cladding to match aesthetics of existing Kingspan KS1000 RW.

**NOTE:**  
Refer to 10106 for proposed photovoltaic panels location and full roof railway system layout.  
To be read in conjunction with 10010.

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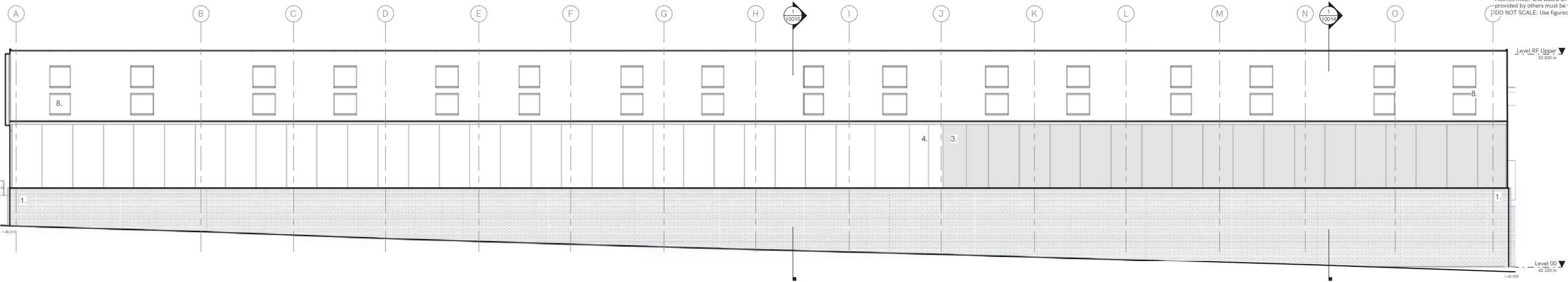
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**Drawing Name**  
Proposed Elevations - East &  
West

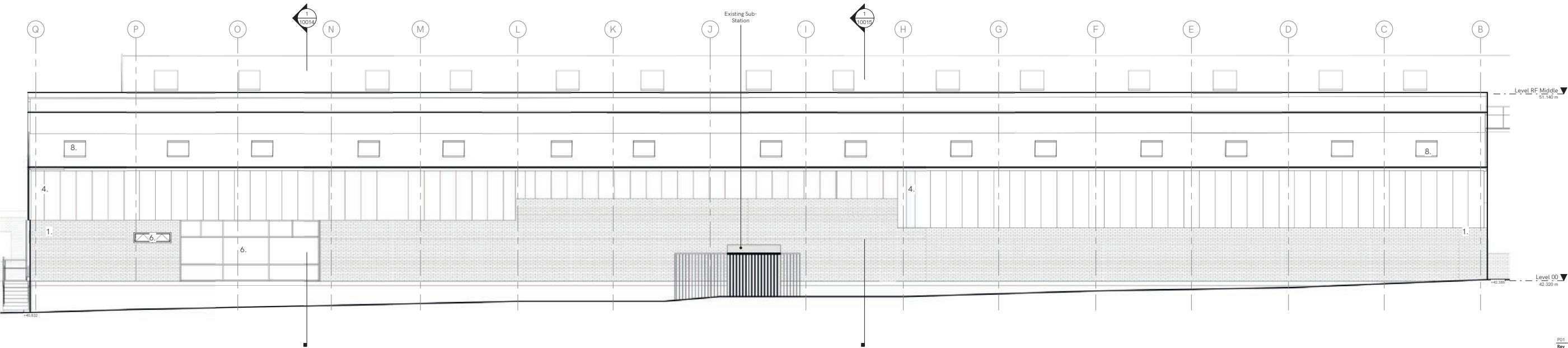
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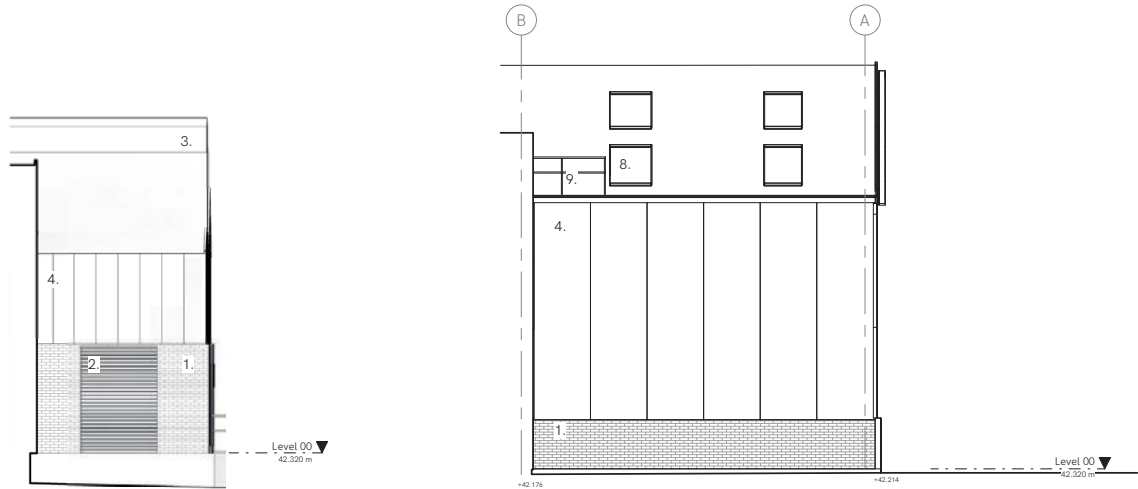




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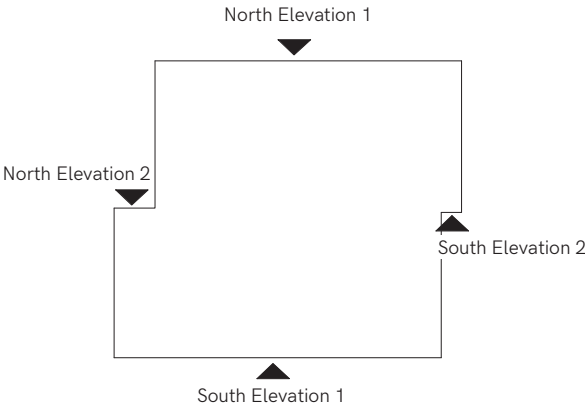


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1 : 100

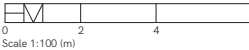


Existing South Elevation 2  
1 : 100

Existing North Elevation 2  
1 : 100

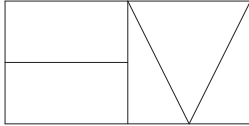


Note:  
Previous versions under drawing -  
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5133-HML-ZZ-ZZ-DR-A-10011



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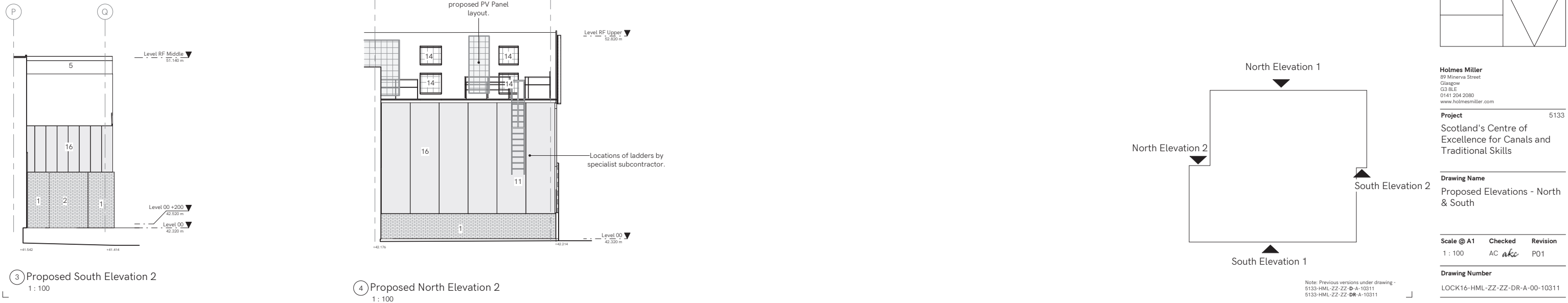
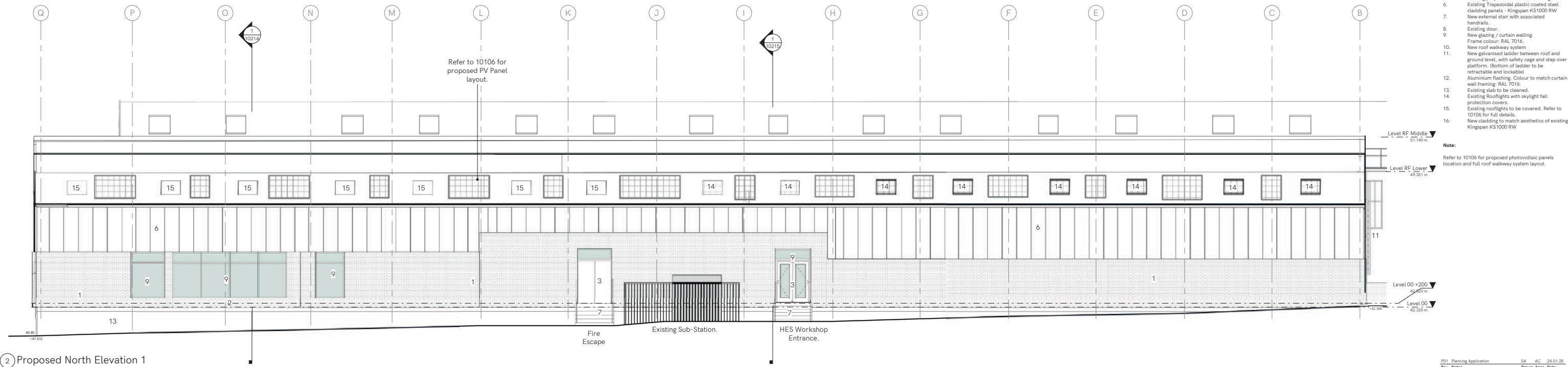
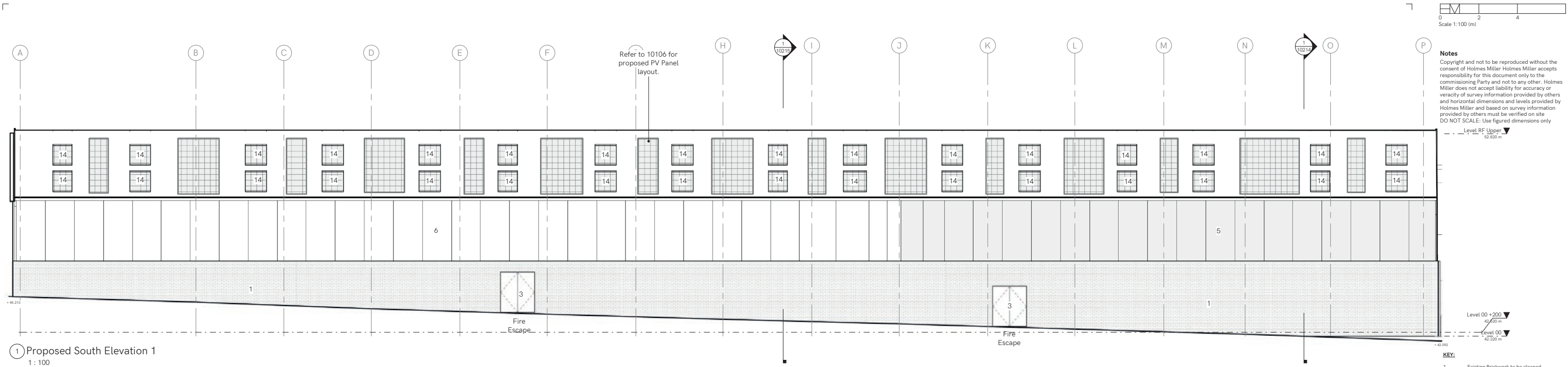
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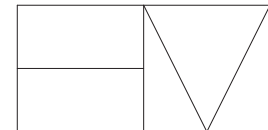
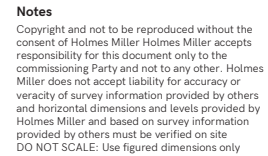
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Existing Elevations - North &  
South

Scale @ A1 Checked Revision  
1 : 100 AC akc P01

Drawing Number  
LOCK16-HML-ZZ-ZZ-DR-A-00-10011







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Drawing Name  
Existing Floor Plan - Level 00

Scale @ A1	Checked	Revision
1 : 200	AC <i>akc</i>	P01

Drawing Number  
LOCK16-HML-ZZ-00-DR-A-00-10004

Note:  
Previous versions under drawing -  
5133-HML-ZZ-00-D-A-10003  
5133-HML-ZZ-00-DR-A-10004



**Note:** All loose FFE is shown for indicative purposes only.

BOH  
CIRCULATION  
MESS AREA  
PLANT  
PUBLIC  
SC WORKSHOP  
STAFF  
STORE  
WORKSHOP

Number	Name	Area
0001	Main Entrance	7 m <sup>2</sup>
0002	Entrance	8 m <sup>2</sup>
0003	Training Room 01	35 m <sup>2</sup>
0004	Training Room 02	36 m <sup>2</sup>
0005	Training Room 03	40 m <sup>2</sup>
0006	Pod 01	6 m <sup>2</sup>
0007	Pod 02	6 m <sup>2</sup>
0008	Acc WC	4 m <sup>2</sup>
0009	Staff Breakout	100 m <sup>2</sup>
0010	Open Plan Office	147 m <sup>2</sup>
0011	Meeting Room 01	28 m <sup>2</sup>
0012	Meeting Room 02	28 m <sup>2</sup>
0013	Meeting Room 03	28 m <sup>2</sup>
0014	First Aid	15 m <sup>2</sup>
0015	SC Corms	15 m <sup>2</sup>
0016	SC Corms	15 m <sup>2</sup>
0017	HES 'Dry Classroom'	48 m <sup>2</sup>
0018	Staff Showers	16 m <sup>2</sup>
0019	Toilets	30 m <sup>2</sup>
0020	Open Plan Store	30 m <sup>2</sup>
0021	M&E Clean Workshop	99 m <sup>2</sup>
0022	Circulation 01	78 m <sup>2</sup>
0023	Circulation 02	26 m <sup>2</sup>
0024	SC Entrance	8 m <sup>2</sup>
0025	SC Circulation	46 m <sup>2</sup>
0026	SC Mess Dining	30 m <sup>2</sup>
0027	Drying Room	18 m <sup>2</sup>
0028	SC Mess Changing	22 m <sup>2</sup>
0029	SC Showers and WCs	28 m <sup>2</sup>
0030	SC Acc Change	6 m <sup>2</sup>
0031	SC Acc WC	4 m <sup>2</sup>
0032	HES Entrance	12 m <sup>2</sup>
0033	HES Circulation	20 m <sup>2</sup>
0034	HES Mess Dining	31 m <sup>2</sup>
0035	HES Mess Changing	29 m <sup>2</sup>
0036	HES Showers and WCs	28 m <sup>2</sup>
0037	HES Acc Change	7 m <sup>2</sup>
0038	HES Acc WC	4 m <sup>2</sup>
0039	Switch Room	17 m <sup>2</sup>
0040	HES Workshop A	1271 m <sup>2</sup>
0041	HES Workshop B	815 m <sup>2</sup>
0042	SC Workshop C	1457 m <sup>2</sup>
0043	SC Workshop Plant	80 m <sup>2</sup>
Combined Room Area		1299 m <sup>2</sup>

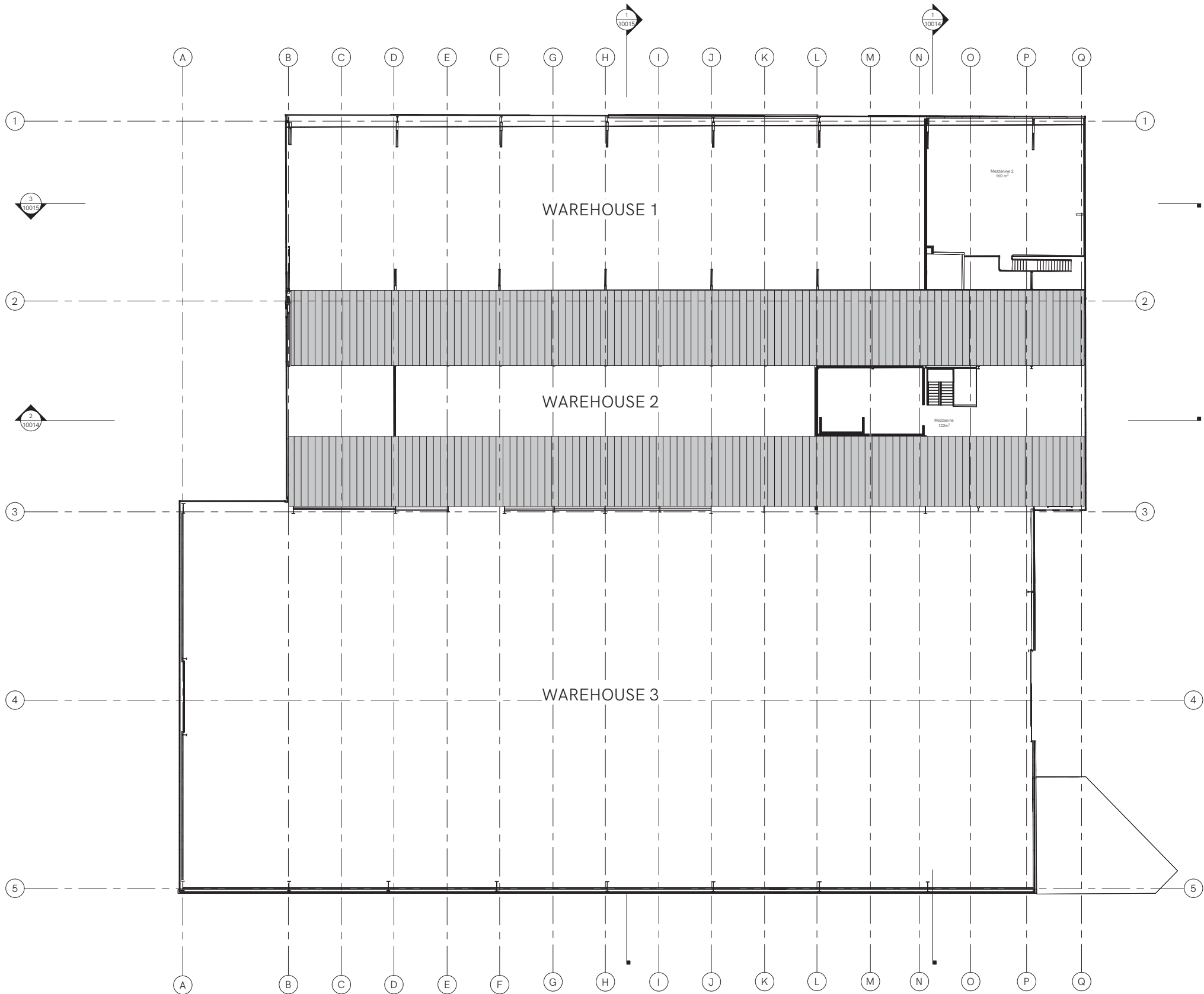
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Proposed Floor Plan - Level  
00

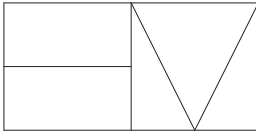
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Note:  
Previous versions under drawing -  
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5133-HML-ZZ-00-DR-A-10103





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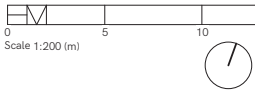
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**Drawing Name**  
Existing Floor Plan -  
Mezzanine Level

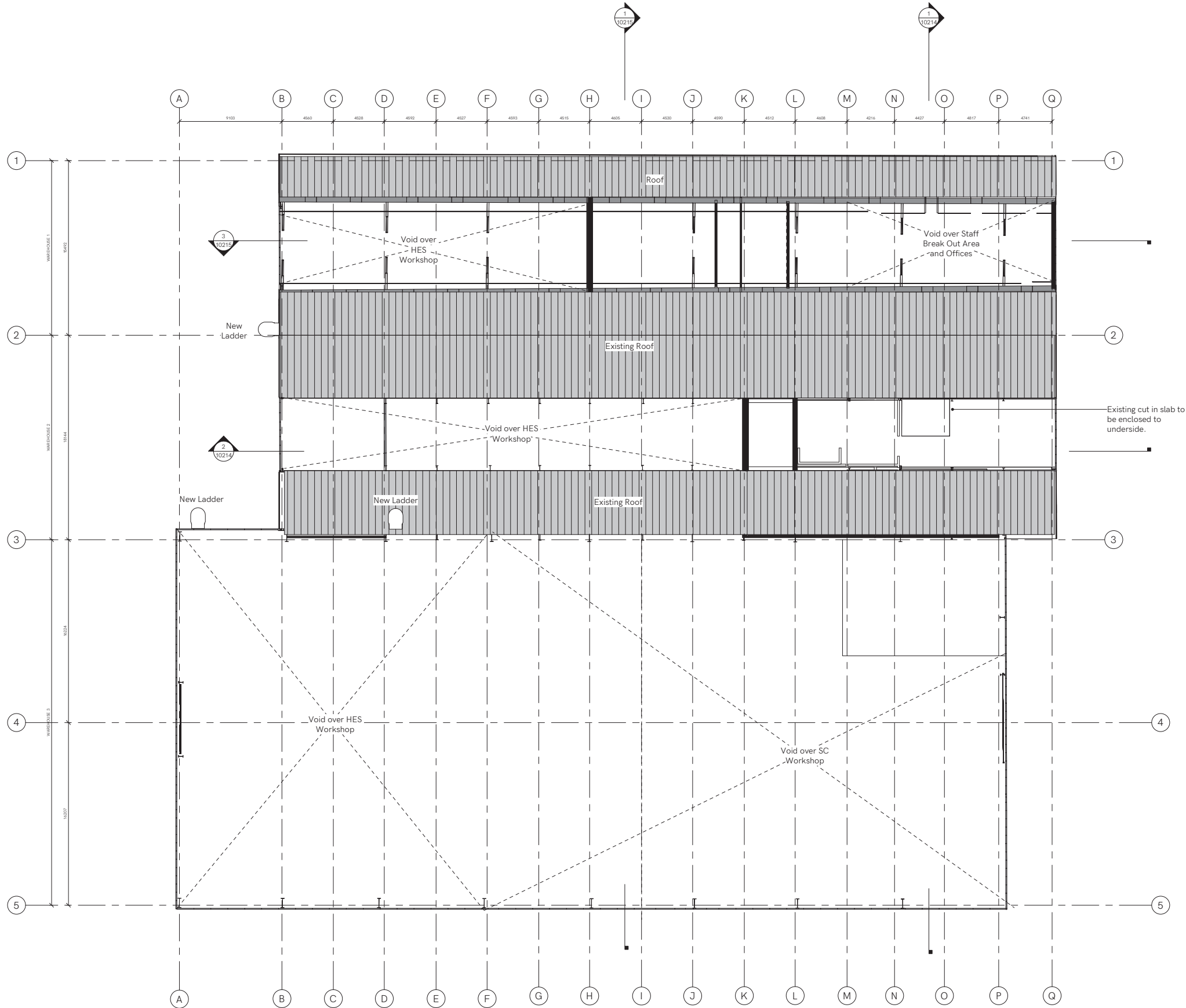
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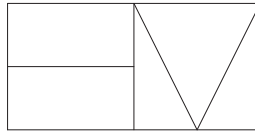
Note:  
Previous versions under drawing -  
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**Drawing Name**  
Proposed Floor Plan -  
Mezzanine Level

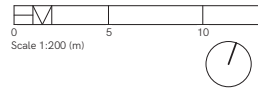
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1 : 200	AC <i>ako</i>	P01

**Drawing Number**  
LOCK16-HML-ZZ-01-DR-A-00-10104

Proposed Floor Plan - Mezzanine  
1 : 200

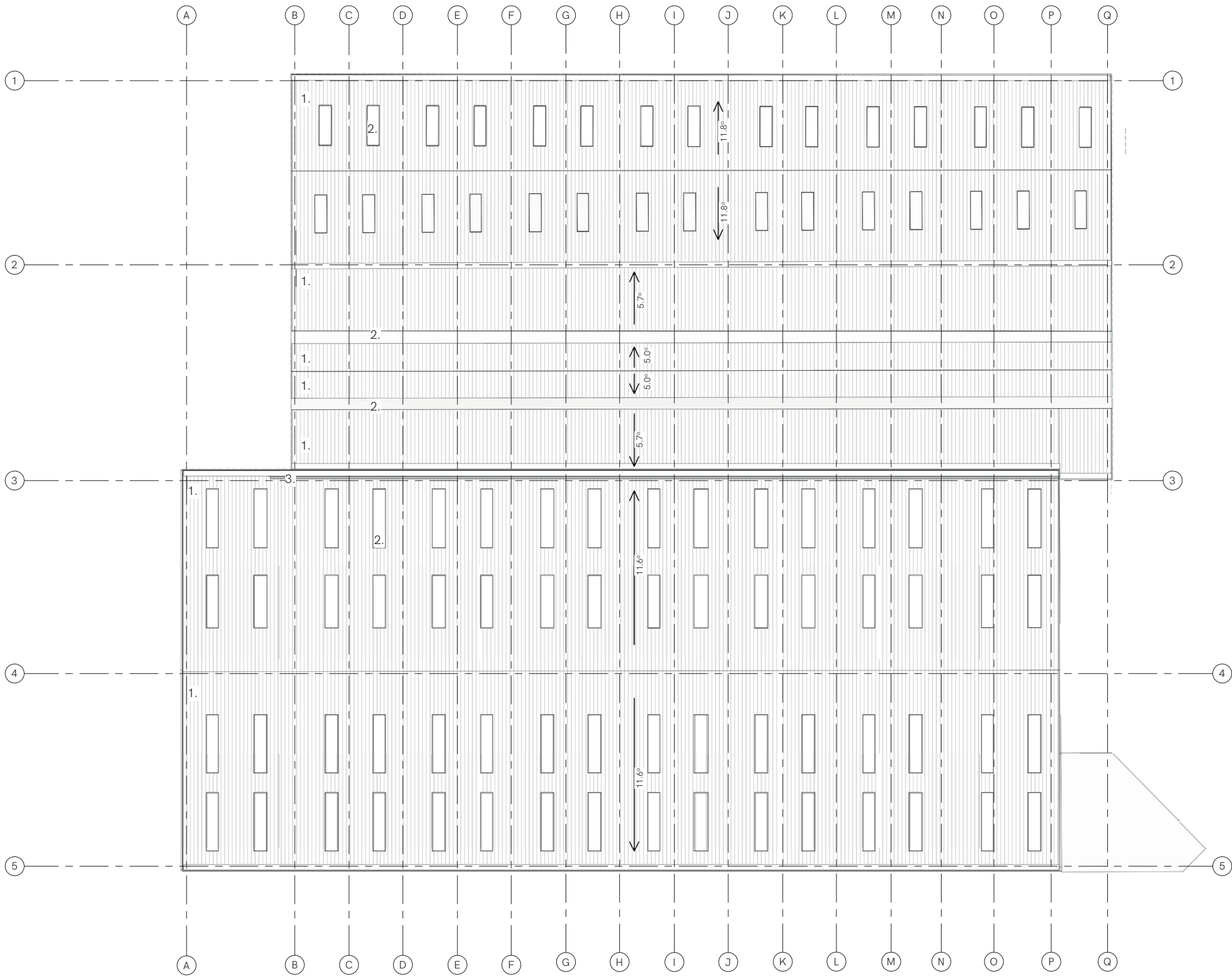
Note: Previous versions under drawing  
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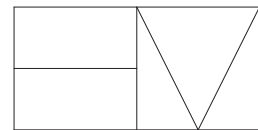
**KEY**  
1. Existing Kingspan composite roof panel system.  
2. Existing polycarbonate rooflight  
3. Existing balustrade.



Roof Plan - Existing  
1 : 200

Note:  
Previous versions under drawing -  
5133-HML-ZZ-RF-D-A-10005  
5133-HML-ZZ-RF-DR-A-10006

P01	Planning Application	SA	AC	24.01.25
Rev	Notes	Drawn	Appr	Date



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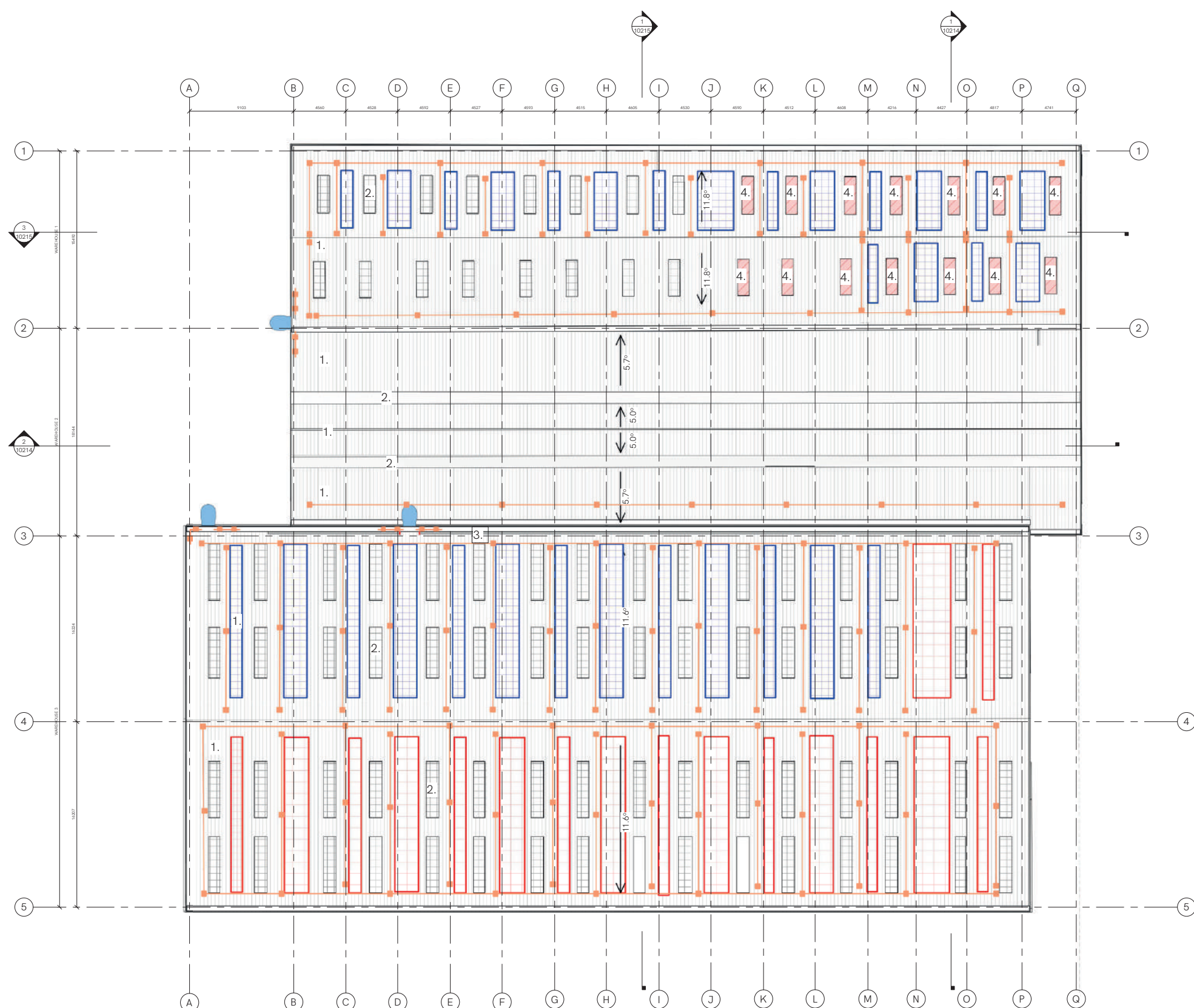
**Project** 5133  
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**Drawing Name**  
Existing Floor Plan - Roof

Scale @ A1	Checked	Revision
1 : 200	AC <i>AKC</i>	P01

**Drawing Number**  
LOCK16-HML-ZZ-RF-DR-A-00-10006





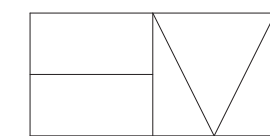
Proposed Roof Plan  
1 : 200



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- KEY**
- Existing -
- 1. Existing Kingspan composite roof panel system
  - 2. Existing polycarbonate rooflight with skylight fall protection covers by simplified design.
  - 3. Existing Balustrade
  - 4. Galvanised ladder between roof and ground level, with safety cage and step over platform. (Bottom of ladder to be retractable and lockable)
- Proposed -
- 4. Existing polycarbonate rooflight covered by composite roof sheeting to match existing.
  - 5. Existing polycarbonate rooflight removed and replaced with new roof light component.
  - SunPower MAX3 400-COM PV Panel Module
  - SunPower MAX5 440-COM PV Panel Module
- NOTE:** To be read in conjunction with drawings by Scorpio Safety specialist sub contractor.

P01 Planning Application	SA	AC	24.01.25
Rev	Notes	Drawn	Appr Date



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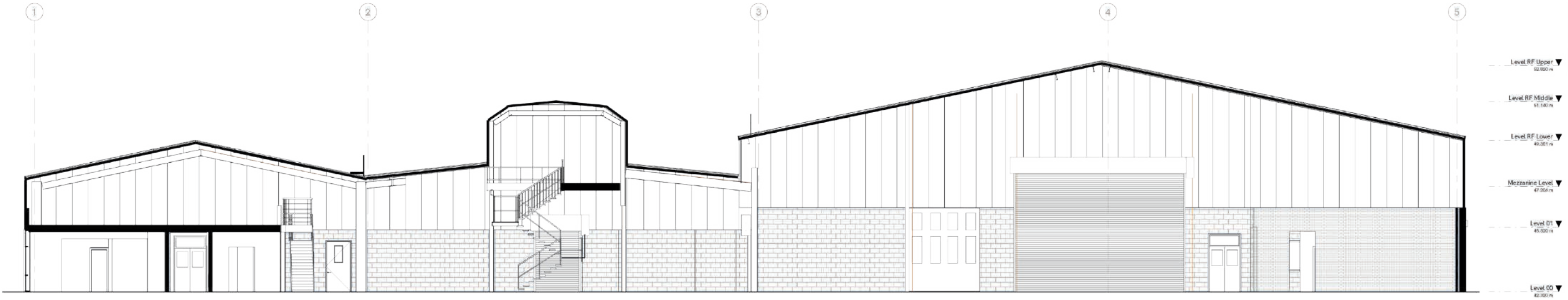
**Drawing Name**  
Proposed Floor Plan - Roof

Scale @ A1	Checked	Revision
As indicated	AC <i>akc</i>	P01

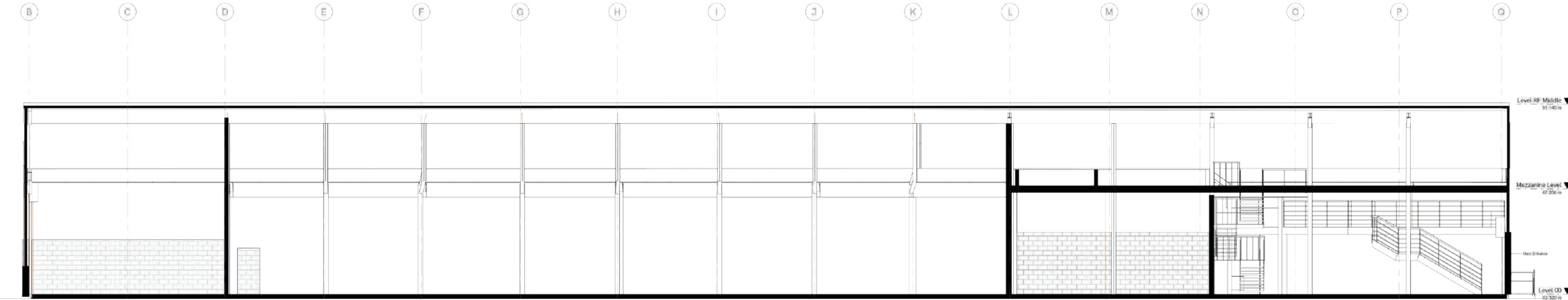
**Drawing Number**  
LOCK16-HML-ZZ-RF-A-00-10106

Note: Previous versions under drawing  
5133-HML-ZZ-RF-D-A-10106  
5133-HML-ZZ-RF-DR-A-10106





Existing Section A-A.  
1 : 100



Existing Section B-B.  
1 : 100

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Rev	Notes	SS	AC	24.01.26
01	Planning Application			



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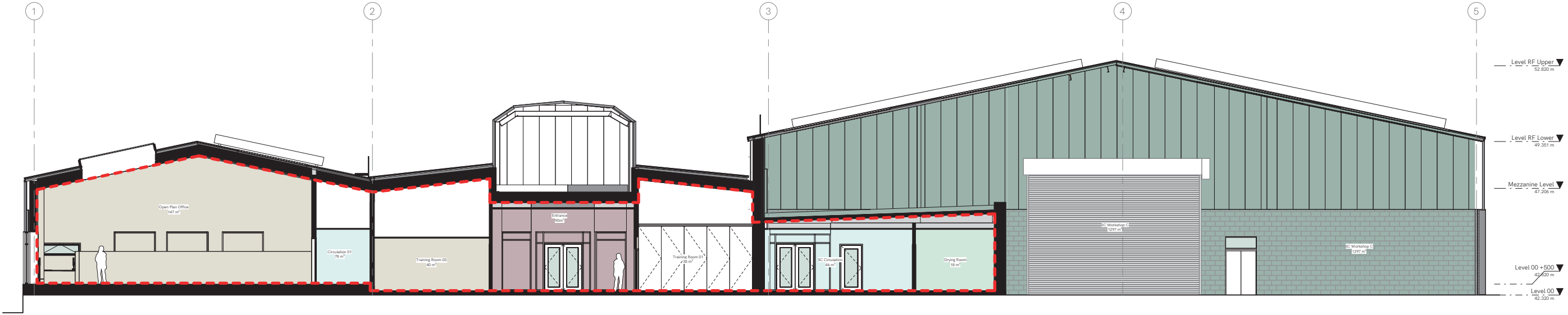
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**Drawing Name**  
Existing Sections  
Sheet 1

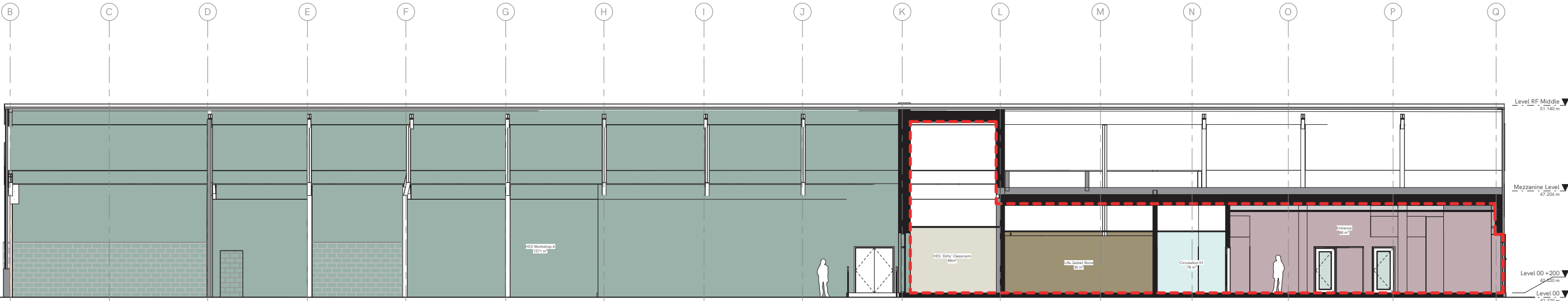
Scale @ A1	Checked	Revision
1 : 100	AC	ake P01

**Drawing Number**  
LOCK16-HML-ZZ-ZZ-DR-A-00-10014

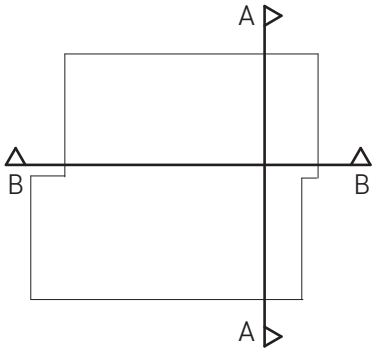
Notes: Previous versions under drawing:  
5133-HML-ZZ-ZZ-D-A-10014  
5133-HML-ZZ-ZZ-DR-A-10014



Section A-A  
1 : 100



Section B-B  
1 : 100

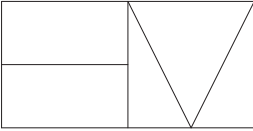


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**KEY**  
--- Thermal Box

P01 Planning Application	SA	AC	24.01.25
Rev	Notes	Drawn	Appr Date



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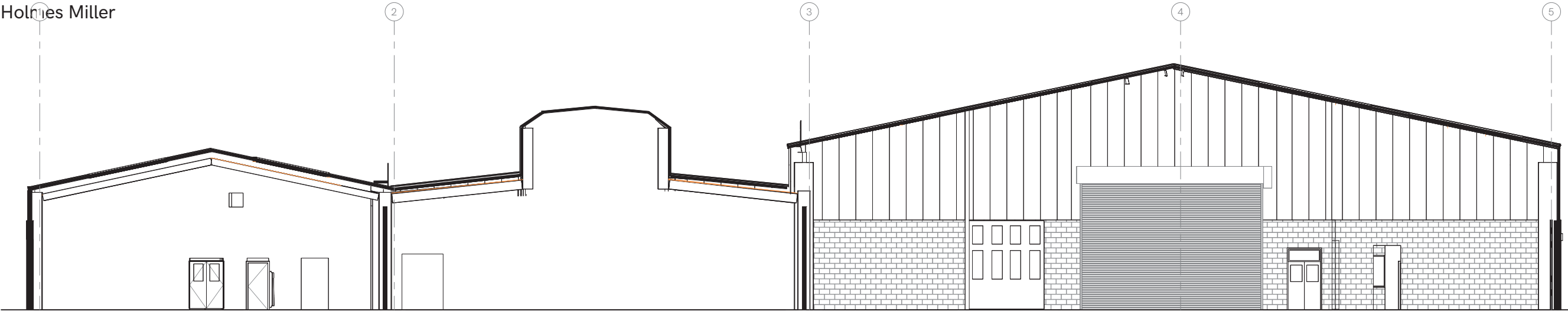
**Drawing Name**  
Proposed Sections - Sheet 1

Scale @ A1	Checked	Revision
1 : 100	AC <i>ake</i>	P01

**Drawing Number**  
LOCK16-HML-ZZ-ZZ-DR-A-00-10214

Note: Previous versions under drawing -  
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Level RF Upper  
52.820 m

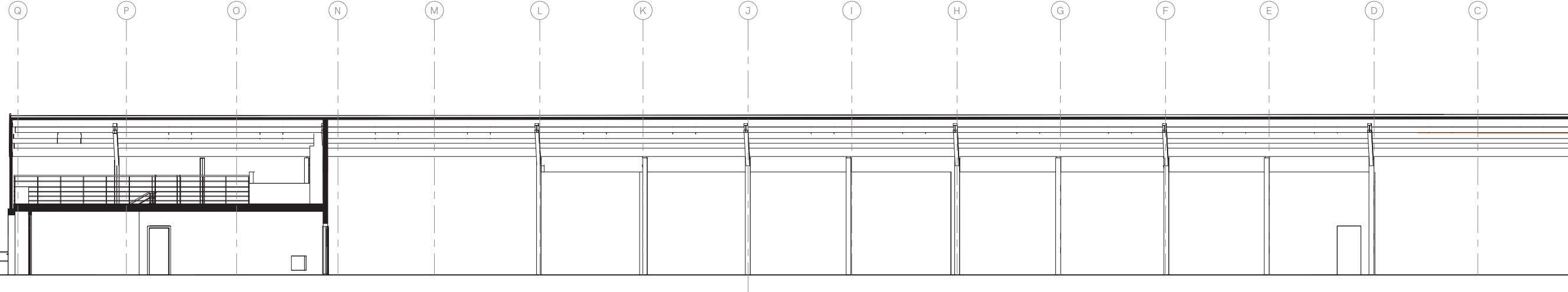
Level RF Middle  
51.140 m

Level RF Lower  
49.351 m

Mezzanine Level  
47.206 m

Level 00  
42.320 m

Existing Section D-D.  
1 : 100



Level RF Middle  
51.140 m

Level RF Lower  
49.351 m

Mezzanine Level  
47.206 m

Level 00  
42.320 m

Existing Section C-C.  
1 : 100



P01 Planning Application SA AC 24.01.25

Rev Notes Drawn Appr Date

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**Drawing Name**

Existing Sections - Sheet 2

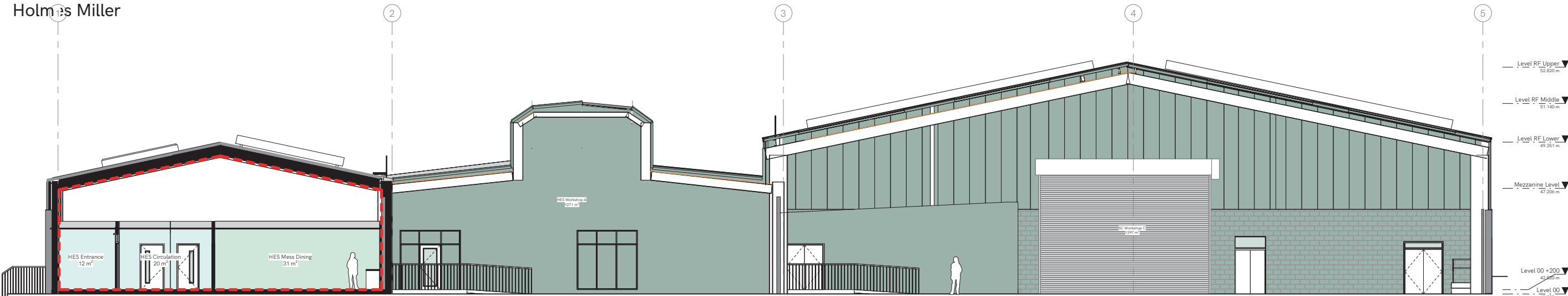
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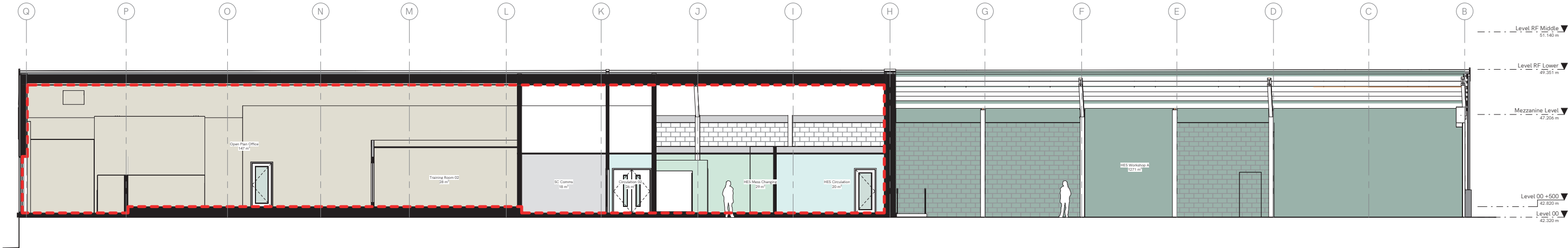
**Revision** P01

**Drawing Number**

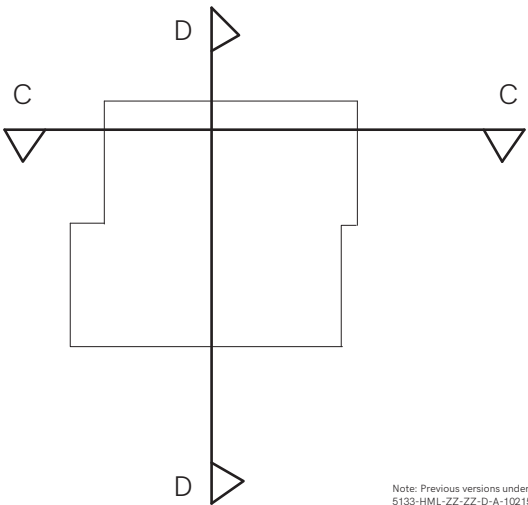
LOCK16-HML-ZZ-ZZ-DR-A-00-10015



Section D-D  
1 : 100



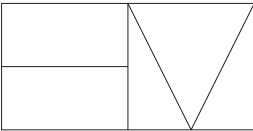
Section C-C.  
1 : 100



Note: Previous versions under drawing -  
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**KEY**  
Thermal Box



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**Drawing Name**  
Proposed Sections - Sheet 2

**Scale @ A1** 1 : 100  
**Checked** AC *aka*  
**Revision** P01

**Drawing Number**  
LOCK16-HML-ZZ-ZZ-DR-A-00-10215



